

**Application for Land Use Consent**

**EntX Digital Screen**

**617 – 649 Colombo Street, Christchurch**

**June 2017**

**Christchurch City Council**

Reference: 111030

Revision: Lodgement



**FORM 9: APPLICATION FOR RESOURCE CONSENT UNDER  
SECTION 88 OF THE RESOURCE MANAGEMENT ACT 1991**

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**TO:** The Christchurch City Council

**Calder Stewart Development Limited**  
PO Box 8356  
Christchurch

hereby applies for Land Use Consent described below.

1. The names and addresses of the owner and occupier (other than the applicant) of land to which this application relates are as follows:  
  
Calder Stewart Development Limited
2. The location of the proposed activity is as follows:  
  
617 – 649 Colombo Street, Christchurch  
Pt Sec 1011, Lot 1 DP 46489, Pt Sec 1009, Lots 1 & 2 DP 7723, Lot 1 DP 11059, Pt Lot 1 DP 7302, Lot 2 DP 7302, Lots 1-4 DP 13211, Lots 3 & 4 DP 495453  
CB19B/966, CB22F/1163, 704572, CB22F/1163, CB19A/1280, CB451/245, CB451/277, CB454/241, CB25A/730, CB501/288, CB501/37, CB501/36, 728053 and 728052.
3. A description of the activity to which the application relates is:  
  
Resource consent is sought for the placement a digital screen on the site located at 617 – 649 Colombo Street.
4. A description of the application site including its location and its natural and physical characteristics.  
  
Section 2 of the application describes the application site and surrounds.
5. Additional Consents  
  
No additional resource consents are needed for the proposed activity.
6. Assessment of Environmental Effects  
  
Section 5 of the application includes an assessment of environmental effects in sufficient detail to satisfy the purpose for which it is required. This is in accordance with the Fourth Schedule of the Resource Management Act 1991.
7. Assessment against Part 2 of the Resource Management Act 1991  
  
Section 7 of the application assesses the proposed activity against the matters set out in Part 2 of the RMA.

8. Assessment against section 104(1)(b)

Section 7 of the application assesses the proposed activity against any relevant provisions of the section 104(1)(b) and in accordance with the Fourth Schedule of the Resource Management Act 1991.

9. Additional Information

We attach any information required to be included in this application by the district plan, the regional plan, the Resource Management Act 1991, or any regulations made under that Act:

- **Appendix A: Approved Resource Consent RMA/2016/2863**
- **Appendix B: Urban Design Panel Report (10 October 2016)**
- **Appendix C: Proposed Signage Design**
- **Appendix D: District Plan Compliance Assessment**



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(Signature of applicant or person authorised to sign on behalf of applicant).

Dated at Christchurch this: 14<sup>th</sup> day of June 2017

**ADDRESS FOR SERVICE:**

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- **Appendix A: Approved Resource Consent RMA/2016/2863**
- **Appendix B: Urban Design Panel Report (10 October 2016)**
- **Appendix C: Proposed Signage Design**
- **Appendix D: District Plan Compliance Assessment**

## QUALITY ASSURANCE

**Urbis Reference:** 111030

**Title:** Application for Land Use Consent – 60 Lichfield Street, Christchurch

**Applicant:** Calder Stewart Development Limited

**Filename:** 111030 170601 Resource Consent Lodgement.docx

**Version:** Lodgement

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**Client Release Review by:** Ray Edwards  
Director



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## 1.0 INTRODUCTION

This proposal seeks to install a digital screen, for the purposes of general advertisement and information sharing (e.g. weather, temperature, time and local events), on the corner of Lichfield Street and Colombo Street, Christchurch. The sign will wrap around the north-eastern corner of the EntX building (that is currently under construction), and hence have frontage to both roads. The sign will be approximately 9m high with a total length of 11.5m (7.5m along Colombo Street & 4m along Lichfield Street). Furthermore, the sign feature moving graphics and images.

Consent is sought due to two non-compliances with the Christchurch District Plan (CDP). The overall activity status is for a **discretionary activity**.

This application addresses the character of the land, the proposed land use activity and the relevant provisions of the Christchurch District Plan. This application has been prepared pursuant to the requirements of Section 88 of the Resource Management Act 1991. Section 88(2)(b) states that an application must be made in the prescribed form and manner; and include, in accordance with Schedule 4 of the Act, as assessment of the environmental effects in sufficient detail to satisfy the purpose for which it is required.

This document has been prepared in accordance with the requirements of the Act and in particular Form 9 and Schedule 4.

### 1.1 Summary of Property Details

Site Address:	617- 649 Colombo Street, Christchurch
Legal Descriptions:	Pt Sec 1011, Lot 1 DP 46489, Pt Sec 1009, Lots 1 & 2 DP 7723, Lot 1 DP 11059, Pt Lot 1 DP 7302, Lot 2 DP 7302, Lots 1-4 DP 13211, Lots 3 & 4 DP 495453
Certificate of Title:	CB19B/966, CB22F/1163, 704572, CB22F/1163, CB19A/1280, CB451/245, CB451/277, CB454/241, CB25A/730, CB501/288, CB501/37, CB501/36, 728053 and 728052
Site Area:	4,488m <sup>2</sup>
Registered Owner:	Calder Stewart Development Limited
District Plan Zoning:	Commercial Central City Business as shown on Planning Map: <i>Draft Central City Zoning, Designations and Other Notations Planning Map Operative and yet to be operative layers</i> . The site is also subject to the following overlays: Central City Core Overlay; Liquefaction Management Area; Central City Building Height 28m Overlay; and Category 3: Lower Noise Level Area.

## 2.0 THE APPLICATION SITE AND SURROUNDS

### 2.1 Site Information

The application site (red outline in Figure 1) is bordered by three road frontages being; Colombo Street to the east, Tuam Street to the south and Lichfield Street to the north.

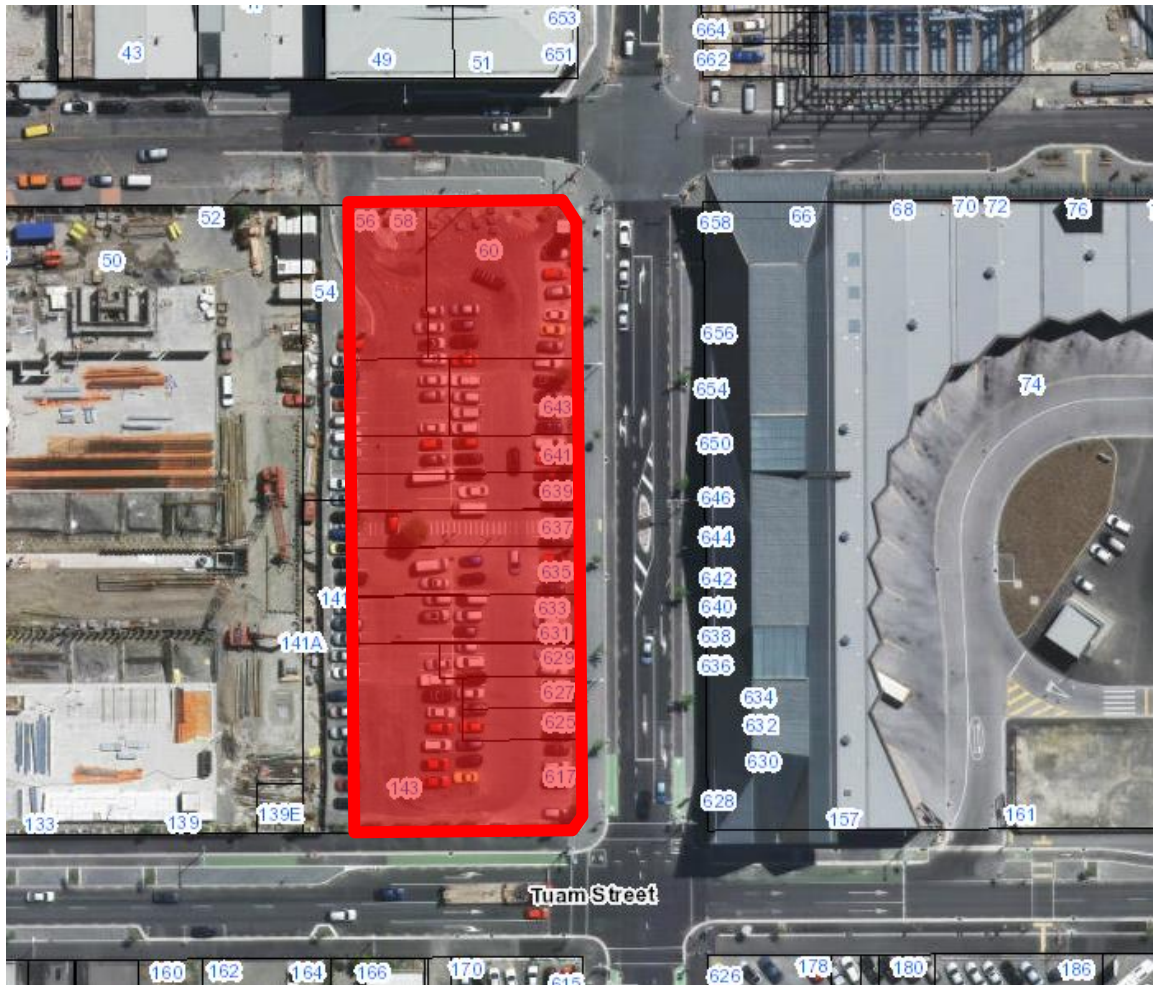


Figure 1: General location of the application site (red) relative to the surrounds

The application site was cleared in late 2014 and subsequently used as a temporary at grade car park. The site is currently undergoing substantial redevelopment with the construction of a three level, cinema and food precinct development, known as Entx. The Entx development will occupy the entire street frontage on western side of Colombo Street between Tuam and Lichfield Street. The approved resource consent (RMA/2016/2863) for Entx is included in this application as **Appendix A**.

## 2.2 Surrounds Information

The application site is zoned Commercial Central City Business and is located within the Central City Inner Zone. The area surrounding the application site was traditionally characterised by office and retail activities. The majority of the surrounding area is at various stages of redevelopment, with the recently opened *Central Bus Exchange* located to the east across Colombo Street, *Ballantynes* and *The Crossing* retail precinct to the east across Lichfield Street, the *Justice and Emergency Precinct* to the west and the “frame” to the south of Tuam Street.

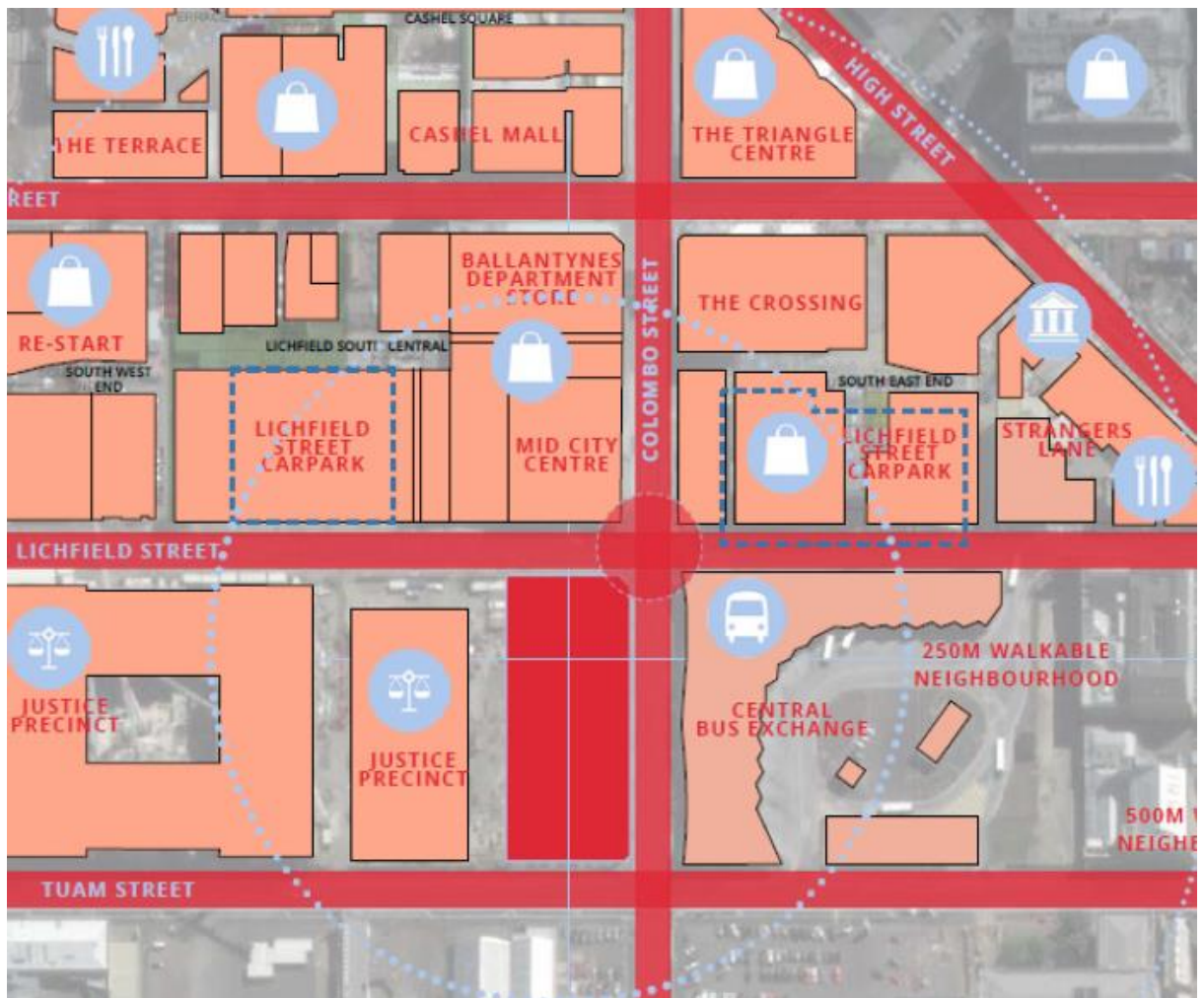


Figure 2: General location of the application site (red) relative to the surrounding land uses

## 2.3 The Road Network

The application site occupies the entire western section of Colombo Street between Tuam Street and Lichfield Street. The proposal seeks to install a screen featuring moving images on the northeast corner of the application site, with frontage to Lichfield Street and Colombo Street. Hence, the characteristics of Colombo and Lichfield Street, and particularly their intersection, are highly relevant to the application.



### Colombo Street

Colombo Street is classified as a local distributor road in the District Plan. The most recent available City Council traffic count data (February 2009) showed that, prior to the earthquakes, Colombo Street carried around 13,000 vehicles per day past the application site (north of St Asaph Street). Current traffic volumes are unknown but are likely to be significantly lower than this based on the comparable reduced scale of commercial activity within the CBD (and probably less than 5,000vpd).

In the vicinity of the application site, Colombo Street has a single traffic lane in each direction separated by a painted centre line. Designated cycle lanes are present on each side of the road separated from traffic lanes by a nib kerb. Additionally, no stopping restrictions extend along the full length of the western side of Colombo Street in this location.

The speed limit on Colombo Street in the immediate vicinity of the application site is 30km/h.

### Lichfield Street

Lichfield Street is also classified as a local distributor road in the Council's central city road hierarchy. The most recent available City Council traffic count data show that, prior to the earthquakes, Lichfield Street carried around 15,000 vehicles per day past the site (east of Durham Street). During this count Lichfield was operating as a one-way street, accommodating eastbound traffic only. However, Lichfield Street was recently converted into a two-way street, providing a single traffic lane in each direction. This change in road function means a significantly reduced traffic flow, compared to what was previously recorded, is anticipated along this street (again probably less than 5,000vpd).

In the immediate vicinity of the application site a no stopping restriction is present along both sides of Lichfield Street.

The speed limit on Lichfield Street in the immediate vicinity of the application site is also 30km/h.

Colombo Street / Lichfield Street Intersection

The Colombo Street / Lichfield Street intersection is adjacent to the location of the proposed billboard. The lane formation of the four approaches can be seen clearly in Figure 3 below. All approaches, except the southbound, consist of two lanes: the northbound approach consists of a right turn only lane and a left turn / through lane; the eastbound approach consists of two unmarked lanes; the southbound approach consists of a left turn lane / through lane; and the westbound approach consists of a left turn only lane and a through lane.



Figure 3: Aerial photograph of Lichfield Street / Colombo Street intersection, approximate location of proposed signage (red), note this diagram is not to scale.

Additionally, the Lichfield Street / Colombo Street intersection features a traffic signal movement that temporarily stops all vehicular traffic, thereby enabling pedestrians to cross the intersection in every direction (including diagonally) simultaneously. The inclusion of this exclusively pedestrian phase in the traffic signal movement is commonly referred to as a Barnes Dance, sometimes called a scramble.

Christchurch City Council conducted its most recent traffic survey (with publicly available data) of the Lichfield Street / Colombo Street intersection on Wednesday the 3<sup>rd</sup> of February 2010 which was also prior to the February 2011 earthquake and the redesign of the intersection.

The recent redesign of the Lichfield Street / Colombo Street intersection means more vehicles are anticipated on the Lichfield Street east approach as the bus lane restriction has been removed. However, a significant directional bias is still anticipated in the eastbound direction.

Current intersection volumes are unknown, however casual observation shows that the intersection operates well below geometric capacity, and the changed function of both Colombo Street and Lichfield Street means that the intersection will not carry anything approaching historic volumes in the future.

## 3.0 THE PROPOSAL

### 3.1 Background

It is important to note that the plans submitted with the approved consent application (RMA20162863) did show a billboard scale sign on the north-eastern corner of the building as per what is currently proposed. It was intended at the time that this sign be a digital screen to be able to provide a variable display. However that consent application did not include the screen because, at that time, actual design details of overall building signage had yet to be resolved.

However, the building signage shown on the previously approved development plans was considered by the Council's Urban Design Panel. Their report, dated 10 October 2016 (**Appendix B**) stated that:

11. Recognises that the signage needs to be developed in more detail and integrated with the scheme – the Panel is not opposed to the LED signage provided its use is associated with the entertainment complex and technical matters including traffic safety can be resolved.

Further, the Section 104 report prepared for RMA20162863 recorded the views of Mr Hugh Nicholson (Council Urban Designer) who stated that "*the signage panels will provide visual interest*" to the site.

Given these initial levels of Council support for what is the now-proposed signage, the Applicant further developed the proposal and has commissioned an experienced signage company to further develop the concept. As a result of this additional work, this application formally seeks consent for the proposed digital screen on the north-eastern corner of the building.

### 3.2 General Description

This proposal relates solely to the digital screen located on the northeast corner of the application site. Any other signage shown on the previously approved plans, if not permitted by the District Plan, will be the subject of a future resource consent application as necessary.

The proposed digital sign will wrap around the north-eastern corner of the building and hence have frontage to both Colombo Street (7.5m length) and Lichfield Street (4m length). The proposed digital sign will protrude from the face of the building by up to 700mm. Additionally, it will be lit and feature moving images for the purpose of advertising and information sharing. The proposed sign will not utilise sound files to accompany any imagery.

The proposed signage shall be partially concealed by the presence of a veranda protruding from the building's façade. This is particularly relevant with regards motorist's view of the sign in close proximity of the Lichfield Street / Colombo Street intersection.

**Appendix C** provides an artist's impression of the proposal.

## 4.0 DISTRICT PLAN ASSESSMENT

### 4.1 Zoning of Application Site

The site is zoned Commercial Central City Business on the Planning Map: *Draft Central City Zoning, Designations and Other Notations Planning Map Operative and yet to be operative layers*.

The proposal only seeks permission for the placement of the digital screen on the northeast corner of the application site. The building upon which the proposed screen is to be mounted has previously been consented by the Council (RMA12016/2863) and is already under construction; hence none of the built form standards in Section 15.4 of the Christchurch District Plan are relevant to this application.

Instead, the rules applicable to this proposal are considered to be District Plan Chapter 6.3 (Outdoor Lighting, and Chapter 6.8 (Signs). A complete compliance assessment has been included in this application as **Appendix D**. Below is a commentary on the key-compliance issues.

### 4.2 Compliance Assessment – Section 6.3 Outdoor Lighting

Rule 6-6.3.6 requires that light spill from the proposed screen be no greater than 20 lux measured 22 metres<sup>1</sup> away from the sign. It is noted from the Council decision on RMA2016/998<sup>2</sup> that:

- *This type of electronic sign is made of many very small light sources in an array. Each one emits very little light and in combination the overall board also doesn't emit light that actually spills over the edges of the board in any significant way at all.*
- *Maximum daytime levels of this type of signage would be 5000 candela/m<sup>2</sup> and night-time would be 250 candela/m<sup>2</sup>*

The candela to lux calculation with a separation distance measured in metres is:

*The illuminance  $E_v$  in lux (lx) is equal to the luminous intensity  $I_v$  in candela (cd), divided by the square distance from the light source  $d^2$  in square meters (m<sup>2</sup>):*

$$E_{v(lx)} = I_{v(cd)} / (d_{(m)})^2$$

Therefore, the daytime illumination of 5000 candela/m<sup>2</sup> level would provide a lux reading of 2.5 measured 22m away from the source, and the night-time illumination of 250 candela/m<sup>2</sup> level would provide a lux reading of 0.13 measured 22m away from the source.

<sup>1</sup> 22 metres is the 20m width of either frontage road plus two metres into the sites located across either frontage road.

<sup>2</sup> An application for an LED billboard at 50 Victoria Street

### 4.3 Compliance Assessment – Section 6.8 Signage

Rule 6.8.4.2.4 permits a total signage area of 95m<sup>2</sup> based on a 190m primary building frontage. The proposed 103.5m<sup>2</sup> sign exceeds this permitted maximum.

Furthermore, Rule 6.8.4.2.4 permits the proposed signage to have a maximum height of approximately 9m above ground level. The top of the proposed digital sign will be approximately 18m above ground level.

Rule 6.8.4.2.5 permits a maximum projection of a 200mm for a sign mounted to the face of a building. The proposed digital sign will project up to 700mm from the face of the building. It is important to note this extent of projection is required to allow access for maintenance of the sign.

Rule 6.8.4.1.4 d) requires digital signage to be considered as a discretionary activity.

Rule 6.8.4.2.2 requires that any sign shall be located so as not to obscure or to detract from the interpretation of any traffic sign or control. The proposed signage is located in the vicinity of the Lichfield Street / Colombo Street intersection traffic controls. However, the billboard is located approximately 9.4m above street level, considerably higher than a standard traffic light. Additionally, a substantial veranda projects out of the building's façades directly below the proposed signage. Thereby, limiting motorist's view of the proposed signage in close proximity of the junction, particularly those on the southern and western approaches. Therefore, the presence of the signage will not obscure or detract from the interpretation of the intersection's traffic controls and as such the proposal is considered to comply with this rule.

### 4.4 Activity Status

The overall activity status of this proposal is for a **discretionary activity**.

## 5.0 ASSESSMENT OF ENVIRONMENTAL EFFECTS

Under Section 104 1) a) of the Resource Management Act (RMA) the consent authority must consider any actual and potential effects on the environment of allowing the activity. Section 104 1) c) extends the extent of discretion further through consideration of any other matter the consent authority considers relevant and reasonably necessary to determine the application. In relation to this particular proposal it is considered that the potential effects relate to character and amenity of the vicinity, light spill, and potential effects on transport safety. Discussing these matters in turn:

### 5.1 Character and Amenity

The proposed digital sign is located within a commercial area, mounted on the exterior of an entertainment centre and surrounded predominantly by retail activity (i.e. Ballantynes to the north) and transport facilities (i.e. the Central City Bus Exchange to the east). Therefore, the digital screen will not appear out of context with the nature of the immediate vicinity and not in close proximity of any heritage buildings, residential properties and/or open spaces.

The proposed digital sign will display a sequence of advertisements and community notices (e.g. temperature, weather, events) and potentially short film clips. The images on screen will be constantly changing as opposed to the traditional static image. This will create an engaging and contemporary environment, aiding in the rejuvenation of the central city and the creation of a commercial hub. Furthermore, the screen will have the ability to provide the surrounding community with relevant, up to date information. This enables the sign to support local events and businesses and to inform the public of current conditions/events.

The large and understated style of the supporting façade makes it a logical location for a feature of this nature. The signage is in keeping with the surrounding architecture, mirroring the supporting building's linear features and sharp angles. The signage will create a point of visual interest for pedestrians in the vicinity and break up the associated façades. Hence, providing a focal point, enlivening the surrounding space and engaging with the active street frontage below.

The proposed digital sign is positioned on a prominent corner of the EntX building, with frontage to both Colombo Street and Lichfield Street. It is of considerable size, 11.5m (7.5m Colombo Street and 4m Lichfield Street) by 9m, however due to its wrap around style it will appear significantly smaller in situ. It is considered that signage of this scale is in keeping with the size of the development it is to be mounted upon, resulting in the signage making a statement complimentary to the building rather than being out of scale.



It is acknowledged the height of the proposed billboard breaches standard 6.8.4.2.4 of the District Plan. However, the proposed digital sign needs to be located above the glass section of the façade and a solid canopy which is positioned at a height of approximately 9.3m above ground level. Additionally, the proposed digital sign does not extend above the height of the supporting building's façade, instead stopping 0.8m below. Therefore, the dimensions and location of the proposed digital sign have been carefully considered in order to respect and compliment the architectural features of the supporting building rather than obstruct or unbalance them.

Finally, the signage will be maintained to a high standard so long as it is in situ. Ensuring the overall impact remains sharp and contemporary.

Overall it is considered that the proposed digital sign will add visual interest to the building as identified by Mr Nicholson. It is considered that there will only be positive character and amenity effects.

## 5.2 Light Spill

From the outset it is important to note that the proposed digital sign installation will comply with Council requirements in relation to potential light spill.

Furthermore, the sites adjacent to the proposed digital sign are all occupied by either retail or transportation facilities; neither of these activities are sensitive to night time light and glare. Central city locations typically experience reasonably high levels of ambient lighting due to the nature of the activities occurring within them.

The proposed sign will contribute to a lively, vibrant, artistic and engaging pedestrian atmosphere in the Central City Core. Featuring animations and graphics that is compatible with the predominantly entertainment and retail uses in the immediate area.

## 5.3 Transport and Pedestrian Safety

Safety is a key factor to be considered with regards the effects of the proposed digital sign, and in particular transport and pedestrian safety in the vicinity of the Colombo Street / Lichfield Street intersection.

This proposal seeks to mount a digital sign approximately 9.4m above street level, with a maximum height of approximately 18.4m. The proposed sign does not block or conceal any traffic signs or/and signals. However, it is located in close proximity of a junction and therefore its impact on the operation of the junction needs to be considered, particularly with regard to the interpretation of traffic signals.

This proposal seeks to install a digital screen on the southwest corner of the Lichfield Street / Colombo Street intersection. Figure 5 below provides an indication of the signs anticipated key view shafts. The figure shows the small area within which the entire screen may be viewed from one position, supporting

the previously expressed opinion that the location and wrap around nature of the sign will significantly reduce both its visual impact and apparent size.

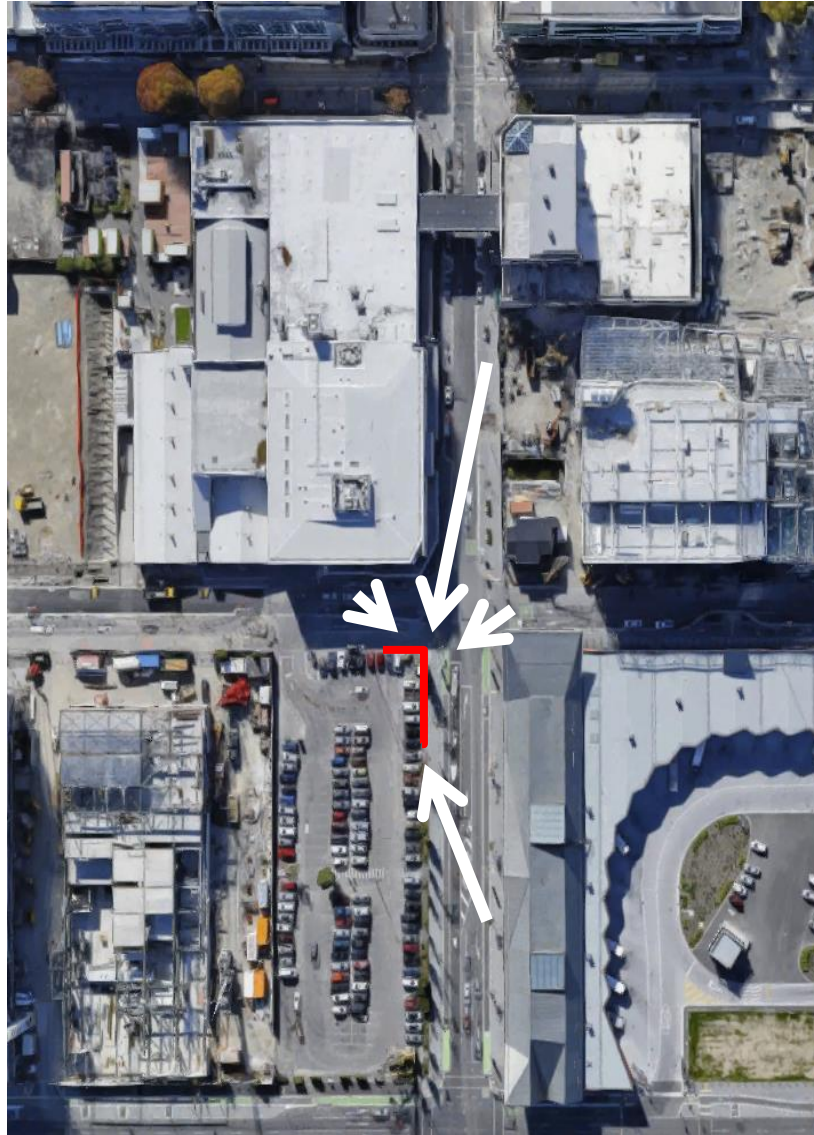


Figure 5: Illustration of Proposed Sign's (red) View Shafts (white arrows), note this drawing is not to scale

In the context of potential traffic effects, and as shown in Figures 6-9 below, the Colombo Street southbound approach is the one principally affected by the installation of the digital sign because as one approaches the intersection the full view of both screen panels will become apparent. In comparison, the northbound Colombo Street and eastbound Lichfield Street approaches only ever have a partial view of the sign and importantly are adjacent to it when entering the intersection; and hence it is not in the driver's line of sight. The westbound Lichfield Street approach only has view of the sign when in the immediate vicinity of the junction.



Figure 6: Street View of Intersection, Colombo Street looking south



Figure 7: Street View of Intersection, Colombo Street looking north



Figure 8: Street View of Intersection, Lichfield Street Westbound



Figure 9: Street View of Intersection, Lichfield Street looking east

Figures 6-9 demonstrate the limited view of the signage from the Colombo Street northbound approach and the Lichfield eastbound approach. As the proposed signage is immediately adjacent to these approaches and therefore is not within the driver's line of sight when interpreting/negotiating the junction.

The Colombo Street southbound approach and the Lichfield Street westbound approach face the signage when approaching the junction and are therefore potentially more impacted. However, drivers are able to priorities the information they are presented with, focusing on relevant information (e.g. traffic signals, obstructions, and hazards) and filtering out inconsequential information (e.g. advertisements, and scenery). Furthermore, the exact location of the signage, specifically in terms of height, dramatically reduces the sign's influence.

Further, the NZTA publication *P43 Specification for Traffic Signals* states:

3.10.3.1 *Lantern mounting height*

*Except where the tertiary or secondary lanterns are mounted within 10m from the vehicle limit line, all vehicle lanterns shall have a mounting height of 4.1m, measured to the top mounting bracket of lantern.*

The above statement provides the standard height of traffic lanterns (4.1m). Therefore, it can be calculated the base of the sign will be approximately 5.3m clear of the junction's traffic signals (assuming standard lantern heights are adopted). Thereby, significantly reducing the likelihood of visual conflict and driver confusion as they are unlikely to viewed in the same line of sight except from a substantial distance.

In terms of pedestrian safety, a similar argument can be adopted. The proposed sign is located so as not to conflict with the view and interpretation of pedestrian signals at the Lichfield Street / Colombo

Street intersection. Pedestrian's will therefore not be confused by the presence of the sign when interoperating the traffic signals and will be able to read the situation effectively and safely. Additionally, the sign is located in a pedestrian friendly area with wide footpaths, hence if an individual wants to watch/read the adverts/information being displayed on the screen they will be able to do so in a safe manner without causing an obstruction.

Overall, the effects on the operation of the Lichfield Street / Colombo Street intersection are anticipated to be less than minor. However, the condition of monitoring the performance of the Lichfield Street / Colombo Street junction post the installation of the digital sign is offered to ensure there are no detrimental effects on transport and pedestrian safety in the immediate vicinity.

#### **5.4 Effects Summary**

The above assessment of effects predominantly focuses on the amenity value of the surrounding area, and the impact on traffic and pedestrian safety at the Lichfield Street / Colombo Street intersection.

It is concluded the amenity value of the surrounding area will not be negatively influenced by the installation of the digital sign. It is considered that the digital sign is in keeping with the surrounding environment both in terms of architectural style and the nature of the activities occurring in the vicinity. Furthermore, the sign is considered to enliven the space and engage with passing pedestrians, displaying a variety of information. This has the positive effect of aiding in the regeneration of the central city.

In terms of traffic, the negative effects were considered to be less than minor due to the placement of the digital screen.

Overall, for the reasons outlined above, it is considered that any adverse effects on the existing environment will be less than minor, and no persons are considered adversely affected by the proposal.

#### **5.5 Consideration of Alternatives**

It is considered any adverse effects of the proposed development on the environment relate solely to the identified non-compliances discussed above. As concluded above the effects associated with the proposal will be less than minor. Accordingly, under Schedule 4 clause 6(1)(b) of the Resource Management Act, there is no need for alternatives to be considered.

## 6.0 ASSESSMENT OF OBJECTIVES AND POLICIES

The preceding sections of this application have considered the relevant rules of the Christchurch District Plan and the relevant assessment matters in association with the effects on the environment. This section of the application assesses the proposal in relation to the relevant objectives and policies of the Christchurch District Plan.

### 6.1 Commercial

#### 15.2.2 Objective – Centres-based framework for commercial centres

- a. *Commercial activity is focussed within a network of centres (comprising the Central City, District Centres, Neighbourhood Centres, Local Centres and Large Format Centres) to meet the wider community's and businesses' needs in a way and at a rate that:*
  - i. *Supports intensification within centres;*
  - ii. *Enables the efficient use and continued viability of the physical resources of commercial centres and promotes their success and vitality, reflecting their critical importance to the local economy;*
  - iii. *Supports the function of District Centres as major focal points for commercial activities, employment, transport and community activities, and Neighbourhood Centres as a focal point for convenience shopping and community activities;*
  - iv. *Gives primacy to the Central City, followed by District Centres and Neighbourhood Centres identified as Key Activity Centres;*
  - v. *Is consistent with the role of each centre as defined in 15.2.2.1 Policy – Role of centres Table 15.1;*
  - vi. *Supports a compact and sustainable urban form that provides for the integration of commercial activity with community activity, residential activity and recreation activity in locations accessible by a range of modes of transport;*
  - vii. *Supports the recovery of centres that sustained significant damage or significant population loss from their catchment, including the Central City, Linwood, and Neighbourhood Centres subject to 15.2.4.3 Policy Suburban centre master plans;*
  - viii. *Enhances their vitality and amenity and provides for a range of activities and community facilities;*

#### 15.2.2.1 Policy – Role of centres

- a. *Maintain and strengthen the Central City and commercial centres as the focal points for the community and business through intensification within centres that reflects their functions and catchment sizes, and in accordance with frame work that:*
  - i. *Gives primacy to, and supports, the recovery of the Central City;*
  - ii. *Supports and enhances the role of District Centres; and*
  - iii. *Maintains the role of Neighbourhood Centres, Local Centres and Large Format Centres*

*As set out in Table 15.1 – Centre's role*

Table 15.1 – Centre’s role

	Role	Centre and size (where relevant)
A.	<p><i>Central Business District</i></p> <p><i>Principal employment and business centre for the City and wider region and to become the primary destination for a wide range and scale of activities including comparison shopping, dining and night life, entertainment activities, guest accommodation, events, cultural activities and tourism activities.</i></p> <p><i>Provides for high density residential activity, recreation activities and community activities and community facilities (including health and social services) as well as civic and cultural venues / facilities (including museums, art galleries).</i></p> <p><i>Serves the district’s population and visitors.</i></p> <p><i>The focus for the district, sub-regional and wider transport services with a central public transport interchange, providing access to large areas of the district and the surrounding districts of Selwyn and Waimakariri.</i></p>	<p><i>Centre: Central City</i></p>

The proposed signage is in keeping with the preceding objectives and policies. Supporting the rejuvenation of the Central City’s commercial centre in a sustainable and effective manner and encouraging the return of businesses to the area.

15.2.4 Objective – Urban form, scale and design outcomes

- a. A scale, form and design of development that is consistent with the role of a centre, and which:
- i. Recognises the Central City and District Centres as strategically important focal points for community and commercial investment;
  - ii. Contributes to an urban environment that is visually attractive, safe, easy to orientate, conveniently accessible, and responds positively to local character and context;
  - iii. Recognises the functional and operational requirements of activities and the existing built form
  - iv. Manages adverse effects on the surrounding environment.

15.2.4.1 Policy – Scale and form of development

- a. Provide for development of a significant scale and form in the core of District Centres and Neighbourhood Centres, and of a lesser scale and form on the fringe of these centres.
- b. The scale and form of development in centres shall:

- i. Reflect the context, character and the anticipated scale of the zoned centre's function;*
- ii. Increase the prominence of buildings on street corners;*

The proposed signage contributes to the reestablishment of an urban environment in the Central City and is therefore in accordance with the above objectives and policies.

#### 15.2.6.3 Policy Amenity

- a. Promote a high standard of amenity and discourage activities from establishing where they will have an adverse effect on the amenity values of the Central City by:
  - i. Requiring an urban design assessment within the Core of the Commercial Central City Business Zone.**

This proposal seeks to install a digital screen in the commercial district of the central city. The screen is of a scale and style in keeping with the surrounding architecture and will provide supporting structure with a modern and engaging finish. This proposal is therefore in line with the previously stated policy. Additionally, this proposal does not require an urban design assessment.

## 6.2 Signage

#### 6.8.2.1 Objectives – Signage

- a. Signage collectively contributes to Christchurch's vitality and recovery by:
  - i. Supporting the needs of business, infrastructure and community activities;*
  - ii. Maintain public safety; and*
  - iii. Enhancing the visual amenity values and character of the surrounding area, building or structures.**

#### 6.8.2.1.1 Policy – Enabling signage in appropriate locations

- a. Enable signage:
  - i. As an integral component of commercial and industrial environments, strategic infrastructure and community activities throughout the Christchurch District;**

#### 6.8.2.1.3 Policy – Managing the potential effects of signage

- a. In considering Policies 6.8.2.1.1 and 6.8.2.1.2, ensure that the size, number, height, location, design, appearance and standard of maintenance of signs;
  - i. Do not detract from, and where possible contribute to, the character and visual amenity of the surrounding area and public realm;*
  - ii. Integrate within the façade of the building, do not detract from the integrity of the building design, and maintain the building as the primary visual element;*
  - iii. Are in proportion to the scale of buildings and the size of the site; and*
  - iv. Enhance the Central City.**



This proposal seeks permission to install a digital sign on the façades of a building in a commercial area of the central city. The primary function of the sign will be advertisement and information sharing, an important aspect of many businesses.

The proposed billboard is of a style and size in keeping with the building it is to be mounted upon. Additionally, once in situ it will well maintained and add visual interest to the area.

Due to the reasons stated above, the proposal is considered to be supportive of Objective 6.8.2.1 and Policies 6.8.2.1.1 and 6.8.2.1.3.

*6.8.2.1.4. Policy – Transport safety*

- a. *Ensure that signs do not cause obstruction and/or distraction for motorists and pedestrians and other road users.*

The proposed signage is located such as to mitigate any potentially negative impacts on safety in the immediate vicinity. Therefore, the proposal is not considered to contradict Policy 6.8.2.1.4.

### **6.3 Objectives and Policies Summary**

It is ultimately concluded that the proposal is not contrary to the relevant objectives and policies of the Christchurch District Plan.

## 7.0 STATUTORY EVALUATIONS

### 7.1 Recovery Strategy

The Recovery Strategy for Greater Christchurch (the *Recovery Strategy*) prepared by CERA under the Canterbury Earthquake Recovery Act became operative on 1 June 2012. It is a statutory document that must be "read together with, and forms part of" other relevant legislation within the greater Christchurch area. The City and District Plans (and a number of other statutory documents) must not be interpreted or applied in a way that is inconsistent with the Recovery Strategy. Only Section 3-8 of the Strategy have statutory effect.

"Recovery" is defined under the CER Act as including both restoration and enhancement (Section 3).

Section 4 identifies the vision for the recovery of Greater Christchurch and supporting goals relating to the six components of recovery. The following goals are of particular relevance to this application:

**Leadership and Integration** – Coordination between public and private sector, and communities to contribute to recovery and future growth by:

- Facilitating a timely and efficient recovery

**Economic** – Revitalise greater Christchurch by:

- Restoring the confidence of the business sector to enable economic recovery and growth
- Identifying and facilitating increased opportunities for early and substantial local and international investment;
- Ensuring a range of employment options

**Built Environment** – Develop resilient, cost effective, accessible and integrated infrastructure, building, housing and transport networks by:

- Prioritising infrastructure investment that contributes during recovery and into the future
- Rebuild infrastructure and buildings in a resilient, cost-effective and energy-efficient manner.

Section 5 of the Recovery Strategy identifies a number of priorities for recovery to address and promote social, economic, cultural and environmental wellbeing. These include:

- People's safety and wellbeing
- Supply of land for recovery needs

The proposal seeks to establish erect a digital sign on the north-eastern corner of the "entertainment exchange" building located at 617-649 Colombo Street. It is considered that this proposed activity is consistent with the commercial environment of the application site and supports the surrounding

community. Similarly the proposal is deemed consistent with the Recovery Strategy, effectively the promoting the identified goals or priorities for recovery.

## 7.2 Land Use Recovery Plan

The Land Use Recovery Plan (LURP) sets a policy and planning framework necessary to:

- Rebuild existing communities
- Develop new communities
- Meet the land use needs of businesses
- Rebuild and develop the infrastructure needed to support these activities
- Take account of natural hazards and environmental constraints that may affect rebuilding and recovery.

The LURP specifies a planning framework to meet the needs of business. As previously mentioned throughout the application the proposed signage is temporary in nature and will not detract from the purpose of the residential zone.

The proposal will support the growth and regeneration of the central city by enlivening the street scene.

Section 23 of the Canterbury Earthquake Recovery Act 2011 states that Council's must not act inconsistently with the Land Use Recovery Plan. As stated above the proposal is not considered to be contrary to the Land Use Recovery Plan and as such, satisfies the requirements of Section 23 of the CER Act.

## 7.3 Regional Policy Statement

The proposal does not raise any issues of significance that are relevant to the Canterbury Regional Policy Statement.

## 7.4 Resource Management Act

In considering this application the purpose of the Resource Management Act should be taken as the primary point of reference. Pre-eminence must be given to Part II, the purpose and principles of the Act.

### Part II - Section 5

The purpose of this Act is to promote the sustainable management of natural and physical resources. Section 5 imposes a duty on consent authorities to promote "*sustainable management*" while avoiding, remedying, or mitigating any adverse effects of activities on the environment.

In regards to Section 5, the proposal is consistent with the enabling component of the Act in that the proposal will be aiding in the efficient and sustainable use of the site, without compromising the quality of the surrounding environment.

#### Part II - Section 6

It is not considered that any part of Section 6 '*Matters of National Importance*' are relevant in this case.

#### Part II - Section 7

Particular regard should also be given to Section 7 "Other Matters":

*(b) The efficient use and development of natural and physical resources:*

*(c) The maintenance and enhancement of amenity values:*

*(f) Maintenance and enhancement of the quality of the environment:*

The proposal aims to install a digital billboard that will enliven the surrounding area and engage with the public...

Overall it is considered that this proposal is in consistent with the intentions of Part II of the Act.

#### Part II - Section 8

Similarly there are no known matters that are considered relevant to the Treaty of Waitangi (Section 8) which need to be taken into account.

#### Part II - Conclusion

Overall it is considered that this proposal is in consistent with Part II of the Act.

#### Sections 104 A- D

Section 104C of the Act states:

*(1) When considering an application for a resource consent for a restricted discretionary activity, a consent authority must consider only those matters over which —*

*(a) a discretion is restricted in national environmental standards or other regulations:*

*(b) it has restricted the exercise of its discretion in its plan or proposed plan.*

*(2) The consent authority may grant or refuse the application.*

*(3) However, if it grants the application, the consent authority may impose conditions under section 108 only for those matters over which —*

*(a) a discretion is restricted in national environmental standards or other regulations:*

*(b) it has restricted the exercise of its discretion in its plan or proposed plan*

In making its decision section 104(1) requires a consent authority to have regard to the following matters:

- 104 (1) a) any actual and potential effects on the environment of allowing the activity*
- 104 (1) b) any relevant objectives, policies, or rules of a plan or proposed plan, regional policy statement or proposed regional policy statement, or national environmental standard (NES) or national policy statement (NPS)*
- 104 (1) c) any other matter considered relevant and reasonably necessary to determine the application.*

As outlined in the preceding assessment of effects, it has been considered that the effects on the environment are less than minor and therefore the test under s104(a) has been met.

It is considered that the application is consistent with the objectives and policies of the Proposed District Plan and can be granted with appropriate conditions imposed under section 108 of the Act.

It is considered that the application is consistent with the objectives and policies of the Christchurch City Plan and Proposed District Plan and can be granted with appropriate conditions imposed under section 108 of the Act.

An assessment of other relevant statutory documents is provided in Section 8 of this application.

Accordingly it is considered that the application may be granted under Section 104C (2) of the Act.

## **8.0 MONITORING**

As the proposal is not considered to generate any significant adverse effects, it is not considered necessary to require any monitoring of the proposal.

## 9.0 CONSULTATION

Clause 6(1)(f) of Schedule 4 to the Resource Management Act states that an Application should include “Identification of the persons affected by the activity, any consultation undertaken, and any response to the views of any person consulted”.

The effects arising from the proposal have been considered in Section 6 of this application and are considered to be less than minor. Given that the effects on the proposal are less than minor we do not consider there are any adversely affected parties and we have not undertaken any consultation.

## 10.0 NOTIFICATION

Under Section 95A(2), a consent authority must publicly notify the application if it decides (under Section 95D) that the activity will have or is likely to have adverse effects on the environment that are more than minor.

The potential effects arising from the proposal have been assessed in Section 6 of this application and it is considered that the proposal will have less than minor adverse effects.

It is therefore not considered necessary to notify this proposal to any other person as the effects of the proposal are contained within the development area. There are no affected persons under Section 95E.

Additionally, there are no special circumstances which exist that would require notification under Section 95A(4).

## 11.0 CONCLUSION

This application relates to the installation of a digital screen on the north-eastern corner of 617-649 Colombo Street. The screen will have frontage to both Litchfield Street and Colombo Street.

The proposed screen is lit and will feature moving images and animation. Therefore, it is classified as a discretionary activity (*D1*) according to the Christchurch District Plan. Additionally, the height and dimensions of the screen result in non-compliance with standard 6.8.4.2.4 *Signs attached to building*. Therefore, resource consent is required for a discretionary activity.

The environmental effects of the proposal were subsequently assessed, with particular emphasis placed on the amenity value of the immediate vicinity and the implications on transport and pedestrian safety. Overall, due to the commercial nature of the surrounding area, the scale and modern style of nearby buildings, and the location of the proposed sign, all associated effects were considered to be less than minor.

Ultimately, the sign was found to be in line with the objectives and policies stated in the Commercial and Signage chapters of the Christchurch District Plan, through supporting the rejuvenation of the Central City Commercial area and the enlivening of the street scene.

In accordance with sections 95, 95A-95F, 104, and 104B of the Act, consent may be granted.

APPENDIX A: Approved Resource Consent RMA/2016/2863



## Application for Land Use Consent

### Calder Stewart Development Limited

617-649 Colombo Street, Christchurch

October 2016

Christchurch City Council

Reference: 111030

Revision: Consent Lodgement (V4)





15/11/2016

**FORM 9: APPLICATION FOR RESOURCE CONSENT UNDER  
SECTION 88 OF THE RESOURCE MANAGEMENT ACT 1991**

**TO:** The Christchurch City Council

**Calder Stewart Limited**

PO Box 8356,  
Christchurch

hereby applies for Land Use Consent described below.

1. The names and addresses of the owner and occupier (other than the applicant) of land to which this application relates are as follows:

Otakaro and Her Majesty the Queen

2. The location of the proposed activity is as follows:

617 -649 Colombo Street, Christchurch

Pt Sec 1011, Lot 1 DP 46489, Pt Sec 1009, Lots 1 & 2 DP 7723, Lot 1 DP 11059, Pt Lot 1 DP 7302, Lot 2 DP 7302, Lots 1-4 DP 13211, Lots 3 & 4 DP 495453  
CB19B/966, CB22F/1163, 704572, CB22F/1163, CB19A/1280, CB451/245, CB451/277, CB454/241, CB25A/730, CB501/288, CB501/37, CB501/36, 728053 and 728052.

3. A description of the activity to which the application relates is:

Resource consent is sought for the establishment of a three storey "entertainment exchange" housing a seven screen cinema complex with associated dining precinct on the site located at 617-649 Colombo Street, Christchurch.

4. A description of the application site including its location and its natural and physical characteristics.

Section 2.2 and 2.4 of the application describes the application site and surrounds.

5. Additional Consents

Any additional consents will be prepared by PDP Environmental Solutions in accordance with ECAN requirements.

6. Assessment of Environmental Effects

Section 6 of the application includes, an assessment of environmental effects in sufficient detail to satisfy the purpose for which it is required. This is in accordance with the Fourth Schedule of the Resource Management Act 1991.

7. Assessment against Part 2 of the Resource Management Act 1991

Section 8 of the application assesses the proposed activity against the matters set out in Part 2 of the RMA.



15/11/2016

8. Assessment against Section 104(1)(b)

Section 8 of the application assesses the proposed activity against any relevant provisions of the section 104(1)(b) and in accordance with the Fourth Schedule of the Resource Management Act 1991.

9. Additional Information

We attach any information required to be included in this application by the district plan, the regional plan, the Resource Management Act 1991, or any regulations made under that Act:

- **Appendix A Development Plans**
- **Appendix B LLUR and Preliminary Site Investigation**
- **Appendix C Design Statement**

-----  
(Signature of applicant or person authorised to sign on behalf of applicant).

Dated at Christchurch this: 11<sup>th</sup> day of October 2016

**ADDRESS FOR SERVICE:**

Urbis TPD Limited  
PO Box 10 318  
CHRISTCHURCH 8145

Attention: **Damienne Donaldson**

Telephone: (03) 963 8724  
E-mail: damienne@urbisgroup.co.nz

**ADDRESS FOR APPLICANT  
(for fees and monitoring purposes):**

Calder Stewart Limited  
PO Box 8356  
CHRISTCHURCH

Attention: **Paul Smith**

Telephone: (03) 338 0013  
E-mail: paul.smith@calderstewart.co.nz

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**Appendix A: Development Plans**

**Appendix B: LLUR and Preliminary Site Investigation**

**Appendix C Architectural Design Statement**

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**QUALITY ASSURANCE**

**Urbis Reference:** 111030

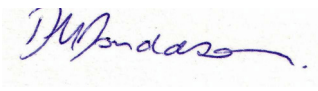
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**Applicant:** Calder Stewart Limited


**Filename:** 111030 161011 Resource Consent Application V4a Lodgement.docx

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**Lodgement Date:** 11<sup>th</sup> October 2016

**Prepared By:** Damienne Donaldson  
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**Reviewed By:** Ray Edwards  
Managing Director 

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## 1.0 INTRODUCTION

The following application relates to the establishment of a three level “entertainment exchange” on the site located at 617-649 Colombo Street. This site is on the western side of Colombo Street and occupies the entire street frontage between Tuam Street and Lichfield Street.

The proposed development comprises a three level building comprising a dining precinct and cinema multiplex. The proposed “entertainment exchange” seeks to offer customers a unique and distinctive experience offering a variety of dining options and movie experiences from the traditional auditoriums, the big screen of the Xtremescreen, to the premier cinema and lux lounge.

The building will comprise:

- A 4,017m<sup>2</sup> ground floor featuring two dining sub-precincts being; traditional “sit down” meals for groups and the other a “hub” of mixed seating, associated entrance lobby’s, back of house facilities and common areas and circulation routes;
- A 3,882m<sup>2</sup> level one which will comprise four cinemas, a food and beverage area, cinema emporium, back of house areas, public amenities, and provision for a 170m<sup>2</sup> future mezzanine.
- Level two will comprise three cinemas and the *lux lounge* within a 2,405m<sup>2</sup> GFA.

The proposed development will comprise a gross floor area across the site of approximately 10,304m<sup>2</sup>.

Consent is sought for a restricted discretionary activity due to non-compliance with four rules of the Operative City Plan.

This application addresses the character of the land, the proposed land use activity and the relevant provisions of the Christchurch City Plan (the Plan). This application also includes an assessment of effects on the environment as required by the Fourth Schedule to the Resource Management Act 1991.



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## 2.0 THE APPLICATION SITE AND SURROUNDS

### 2.1 Summary of Property Details

Site Address:	617- 649 Colombo Street
Legal Descriptions:	Pt Sec 1011, Lot 1 DP 46489, Pt Sec 1009, Lots 1 & 2 DP 7723, Lot 1 DP 11059, Pt Lot 1 DP 7302, Lot 2 DP 7302, Lots 1-4 DP 13211, Lots 3 & 4 DP 495453
Certificate of Title:	CB19B/966, CB22F/1163, 704572, CB22F/1163, CB19A/1280, CB451/245, CB451/277, CB454/241, CB25A/730, CB501/288, CB501/37, CB501/36, 728053 and 728052.
Total Site Area:	4,488m <sup>2</sup>
Registered Owner:	Otakaro and Her Majesty the Queen

### 2.2 Site Information

The 4,488m<sup>2</sup> application site is located at 617-649 Colombo Street, Christchurch. The application site is legally defined as Pt Sec 1011, Lot 1 DP 46489, Pt Sec 1009, Lots 1 & 2 DP 7723, Lot 1 DP 11059, Pt Lot 1 DP 7302, Lot 2 DP 7302, Lots 1-4 DP 13211, Lots 3 & 4 DP 495453 and is held in Certificates of Title CB19B/966, CB22F/1163, 704572, CB22F/1163, CB19A/1280, CB451/245, CB451/277, CB454/241, CB25A/730, CB501/288, CB501/37, CB501/36, 728053 and 728052.

The application site (blue outline) is bordered by three road frontages being; Colombo Street to the east, Tuam Street to the south and Lichfield Street to the north. The aforementioned Justice and Emergency precinct borders the site to the west as shown in Figure 1 on the next page.

The application site is currently utilised as a Christchurch City Council public car park in association with Re:start with provision for 144 spaces with ingress and egress via Lichfield Street only.

### 2.3 Surrounds Information

The application site is zoned Central City Business (Core) with the surrounding area traditionally characterised by retail and office activities. Much of the surrounding area is currently undergoing redevelopment with recent development including the *Central Bus Exchange* to the east across Colombo Street, *Ballantynes* and *The Crossing* retail precinct to the north and northeast across Lichfield Street, the *Justice and Emergency Precinct* to the west and the "frame" to the south on Tuam Street.



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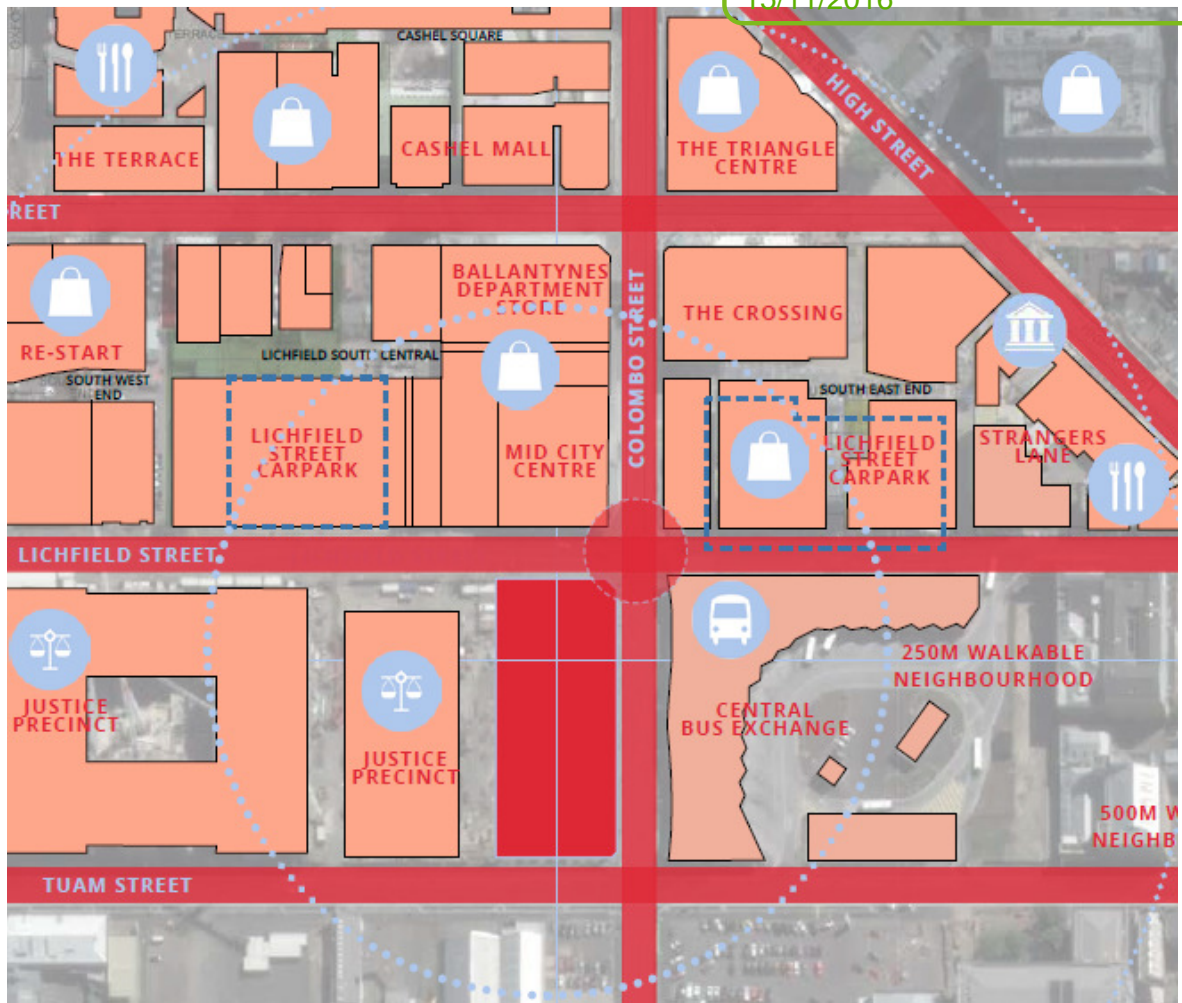


Figure 1: General location of the application site relative to the surrounds and road network.



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## 3.0 THE ROAD NETWORK

### 3.1 Colombo Street

Colombo Street is classified as a local distributor road in the City Plan (Appendix 4b Transport Environments). The most recent available City Council traffic counts reveal that, prior to the earthquakes, Colombo Street carried around 13,000 vehicles per day past the site (north of St Asaph Street).

In the general vicinity of the application site, Colombo Street has a single traffic lane in each direction separated by a painted centre line. Defined cycle lanes are located on each side of the road separated from the traffic lanes by a nib kerb. No stopping restrictions extend along the full length of the western side of Colombo Street in the vicinity of the application site.

Immediately adjacent to the application site, on the northbound approach to the Colombo/Lichfield Street intersection, Colombo Street is marked with shared single lane allowing left and right turns and through traffic whilst the southbound lane is provided with a single traffic lane.

### 3.2 Lichfield Street

Lichfield Street is also classified as a local distributor road in the Council's central city roading hierarchy (Appendix 4b Transport Environments). The most recent available City Council traffic counts (2009) reveal that, prior to the earthquakes, Lichfield Street carried around 15,000 vehicles per day past the site (east of Durham Street).

Lichfield Street has recently been converted from a one way eastbound road to providing a single traffic lane in each direction. This change in road function an anticipated much reduced future traffic flow along the street.

Immediately adjacent to the application site, on the eastbound approach to the Colombo/Lichfield Street intersection, Lichfield Street is provided with a single shared lane allowing left and right turns and through traffic whereas the westbound lane has a dedicated left turn traffic lane and a shared through and right turn traffic lane. A no-stopping restriction runs along the both sides of Lichfield Road outside the site.

### 3.3 Tuam Street

Tuam Street is classified as a main distributor Road in the Council's central city roading hierarchy (Appendix 4b Transport Environments). The most recent available City Council traffic counts (2009) reveal that, prior to the earthquakes, Tuam Street carried around 8,000 vehicles per day past the site (west of Manchester Street).

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Tuam Street has recently been converted from providing a single traffic lane in each direction to a one way eastbound road and has two traffic lanes running west to east. This change in road function may, in the longer term, result in traffic flows along the street exceeding 10,000vpd.

Figure 2 below shows the road layout on Tuam Street outside the application site. Immediately adjacent to the application site, on the eastbound approach to the Colombo/Tuam Street intersection, Tuam Street is provided with a two dedicated through lanes and dedicated left and right turn lanes. On-street parking is not available along this site frontage and instead the road shoulder area provides a median separated eastbound cycle lane.

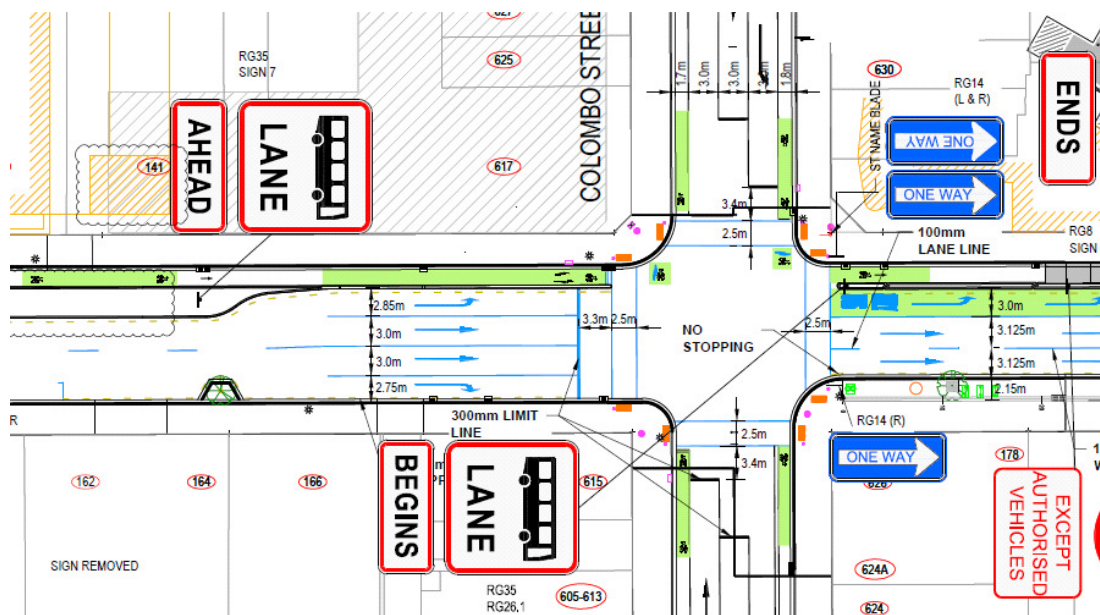


Figure 2: Layout of Tuam Street outside the application site.

### 3.4 Public Transport

The site is located directly opposite the central bus exchange and therefore extremely well served by public transport.

### 3.5 Provision for Cycling

The site is also extremely well served by cycle facilities. As noted earlier, both the Tuam Street and Colombo Street frontages have dedicated cycle lanes provided within the road reserve areas. The bus exchange also provides 87 covered and secure cycle parking spaces within the southwest corner of the building, with another 20-30 cycle spaces provided within road reserve on the three road frontages of the bus exchange.



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## 4.0 THE PROPOSAL

### 4.1 General Description

This application seeks consent to establish an “entertainment exchange” comprising a seven screen cinemaplex with capacity for up to 912 guests with two associated dining precincts. The proposed development will comprise a gross floor area across the site of approximately 10,304m<sup>2</sup> spread across three levels. The proposal is described in detail below and is further illustrated by the development plans attached as Appendix A to this application.

### 4.2 Proposed Building

#### Ground Floor

A 4,017m<sup>2</sup> GFA ground floor featuring a dining and food precinct adjoining each of the three road frontages with a combined 2,791m<sup>2</sup> leasable area. The balance 1,226m<sup>2</sup> of the ground floor comprises; entrance lobbies, back of house facilities, common areas and circulation routes and public and staff amenity areas.

A key feature of the building is the triple height glass atrium located in the north-eastern corner of the building which provides the main pedestrian entrance to the building and a stimulating and welcoming interior. This feature not only instigates a sense of space and light, it also provides a visual connection with the external environment and importantly illuminates the streetscape.

The proposed building also features an undulating veranda roofline reaching a 10m high apex, this coupled with the reduced roofline along the Lichfield Street frontage accentuates the dominant view shaft (Lichfield /Colombo St corner) and provides maximum “exposure to sunlight on the northern edge and a welcoming beacon towards the city after nightfall”.

A second centrally located access is provided along the Colombo Street frontage. Additional pedestrian entrances are located along the Lichfield and Tuam Street frontages with direct access to the dining outlets.

A centrally located escalator, staircase and lift will provide linkages to each of the levels within the building; stairs are also located to the north and south of the public floor area.

#### First Floor

Level one will comprise four cinemas with auditorium seating with cinemas 1 and 4 extending through to level two, a ticket booth, an additional food and beverage area, cinema emporium, back of house areas, administration offices, public amenities, and provision for a future mezzanine. The first floor comprises a GFA of 3,882m<sup>2</sup> split 910m<sup>2</sup> towards food and beverage activities and 2972m<sup>2</sup> towards the Cinemaplex activity.



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## Second Floor

Level two will comprise three cinemas, two of which are lux theatres and a lux lounge which features a bar and lounge area as well as providing in-cinema service. The second floor comprises a GFA of 2,405m<sup>2</sup>.

Table 1 below summarised the proposed floor areas and intended uses:

<b>F&amp;B ACTIVITY</b>	<b>F&amp;B AREA</b>	<b>CINEMA AREA</b>	<b>CINEMA ACTIVITY</b>	<b>FLOOR TOTAL</b>
<b><u>Ground floor</u></b>				
F&B - stage 1	2243	155	Hoyts (boh)	
F&B - stage 2	548	224	Hoyts (egress)	
F&B common services areas	412			
Shared (boh)	121			
Shared (circulation)	289			
Shared (riser)	25			
<b>Ground floor total</b>	<b>3638</b>	<b>379</b>		<b>4017</b>
<b><u>First floor</u></b>				
f&b - stage 1	468	2423	Hoyts	
future mezzanine	316	102	Hoyts (additional boh)	
shared (circulation)	56	136	Hoyts (admin)	
shared (riser)	70	25	Hoyts (boh)	
		141	Hoyts (circulation)	
		38	Hoyts (egress)	
		107	Hoyts (wc)	
<b>Level 1 total</b>	<b>910</b>	<b>2972</b>		<b>3882</b>
<b><u>Second floor</u></b>				
		2308	Hoyts	
		59	Hoyts (boh)	
		38	Hoyts (egress)	
<b>Level 2 total</b>	<b>0</b>	<b>2405</b>		<b>2405</b>
<b>Overall F&amp;B total</b>	<b>4548</b>	<b>5756</b>	<b>Overall Hoyts total</b>	<b>10304</b>

Table 1: Proposed building floor areas.

## 4.3 Parking Provision

It is not proposed to provide any vehicle or visitor parking provision on the site.



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#### 4.4 Loading Provision

It is not proposed to provide parking space for any goods delivery vehicle or rubbish removal vehicle on the site as there is no District Plan requirement to do so.

Instead it is proposed to provide a loading bay area and general back of house facility within the southwest corner of the building. It is proposed that service vehicle parking will be provided via an on street loading zone on Tuam Street adjacent to the entrance to the back of house service area with all inwards goods and outwards goods being of a palletised or containerised nature to enable manual handling between the building and the proposed on-street loading area.

The provision of this on-street zone will require modification of the existing median island located between the eastbound traffic lanes and the eastbound cycle lane outside the site. Preliminary discussions have been held with Council (Mike Calvert) and while the final details of this facility are yet to be determined, at this stage there appears to be no impediment toward providing such an on-street loading facility.

#### 4.5 Traffic Generation

It is important to note that there are not rules relevant to the central city that require the assessment of the proposal in relation to traffic generation. Instead the following information is provided for initial guidance for the purpose of assessing developer contributions for the proposed activity.

It is important to note that it is not possible to estimate with any accuracy the traffic generation of the proposal because no survey data has been found of the traffic generation of other central city cinema complexes.

For information purposes only; Table 2 on the next page provides survey data sourced from the Trips and Parking Database (TPD) for three Auckland suburban-mall based Cinema complexes. This data suggests that the proposed 912-seat cinema complex could generate around 784-2335 trips per day. This generation range is significant and it is also noted that there is no correlation in the dataset between GFA, seat numbers and trip generation. It is also noted that all three sites, being suburban mall based, are provided with substantial parking areas that would encourage use of private car transport. In comparison, the application site is central city based and located directly opposite the bus exchange. These factors will encourage trip linking and alternate transport modes and as such the likely traffic generation of the proposed Cinemaplex will likely be at the lower end of the range suggested by the TDB database data – say around 750 trips per day.



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Site No.	Source of Data	Activity Name	Date of Survey	GFA (m <sup>2</sup> )	Seats	Parking Spaces On-site	Total Daily Trips	Daily trips per seat	Trips for 912 seats
35	ATLA Study	Village 6 Cinemas	Saturday 15/05/1993	2926	1298	447	3367	2.59	<b>2335</b>
41	ATLA Study	Village 8 New Lynn	Friday 28/05/1993	3674	1960	700	1708	0.87	<b>784</b>
93	ATLA Study	Village Cinemas	Saturday 12/02/1994	2376	1300	291	1851	1.42	<b>1281</b>

Table 2: Estimated Trip Generation of proposed 912-seat Cinemplex based on TPD dataset.

In relation to this specific site, it is proposed that Hoyts will operate the cinemas and that have advised that they expect peak daily admissions to be around 4,000 patrons per day and with 1500-2500 admissions on a typical trading day. Adopting 2500 admissions per day, and assuming 33% by private car on a special trip with an average of 2.4 persons per car, then this estimation methodology suggests a typical daily trip generation of around 700 trips per day.

The trip generation of the restaurant areas would be additional to this. Table 3 on the next page provides a sample dataset from the TPD database of restaurants and bars located in suburban locations within the wider Auckland area.

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Site No.	Source of Data	Suburb or Locality	Activity Name	Date of Survey	GFA (m <sup>2</sup> )	Seats	Daily Trips	Trips per 100m <sup>2</sup>
47	ATLA Study	Northcote	Pizza Hutt	Fri 18 Jun 1993	220	99	173	78.6
66	ATLA Study	Glen Eden East	Valentines and The Boats Inn	Fri 22 Oct 1993	980	320	192	19.6
77	ATLA Study	New Windsor	Richardsons Tavern	Fri 10 Dec 1993	1067	350	1123	105.2
78	ATLA Study	CBD	Sizzlers	Sat 18 Dec 1993	860	255	600	69.8
88	ATLA Study	East Tamaki Heights	Metro Bar and Grill	Fri 28 Jan 1994	372	100	210	56.5
90	ATLA Study	Ellerslie	Ellerslie Oaks	Fri 4 Feb 1994	2010	100	1182	58.8
<b>Average</b>								<b>64.8</b>

Table 3: Surveyed Trip Generation of proposed restaurant/bar areas based on TPD dataset.

If the average trip generation rate of 64.8 trips per 100m<sup>2</sup> GFA from Table 3 above is applied to the 4548m<sup>2</sup> GFA associated with the proposed food and beverage activities then this results in an estimated generation of around 2945 trips per day. This estimate seems high relative to that for the Cinemaplex and again it is noted that the survey data is based on suburban locations.

Further guidance for the traffic generation of the proposed food and beverage activities has been obtained from the Councils Developer Contributions Policy document (DCP). The HUE rate for restaurants is 0.0155 and if this rate is applied to the 4548m<sup>2</sup> GFA associated with the proposed food and beverage activities then this results in an estimated generation of around 705 trips per day. If added to the estimated generation for the Cinemaplex (700 trips from earlier), and assuming no trip linking with activities within the site, then the estimated generation is around 1,400 trips per day.

A final comparison has then been made against the data provided in Table A4.12 of the DCP. The base trip rate for Central City & Central City Edge developments is 14.35 trips per 100m<sup>2</sup> GFA which equates to 1,480 trips per day for the 10,304m<sup>2</sup> development.

On the basis of the above it is broadly estimated that the proposed development will generate around 1400 trips per day.





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#### 4.6 Signage

The proposal seeks to erect site specific signage associated with the entertainment exchange however, at time of lodgement the details of signage have not been finalised and any signage over otherwise permitted levels does not form part of this consent.

For information purposes only and as indicated on the elevations (Appendix B), signage is likely to comprise of an internally illuminated wraparound LCD screen on the north-eastern corner of the building with additional signage on the northern, eastern and western building elevations.

The location and sizes of the proposed signage are currently being considered and will be controlled to provide a cohesive and consistent design and appearance on the site. Once the details and technical requirements have been finalised if signage should not comply with the relevant City Plan provisions a separate consent will be lodged.

#### 4.7 Provisions of the NES (National Environmental Standards)

The NES standard became operative on the 1st January 2012 and seeks to ensure that land affected by contaminants in soil is appropriately identified and assessed before its developed and if necessary the land is remediated or contaminants contained to make the land safe for human use.

The application site has been checked against ECAN Listed Land Use Register (LLUR) to see whether the site has any recorded history of hazardous activities. The LLUR reports provide an summary of site investigations of the site. This site history for 617-649 Colombo Street (which is included as **Appendix B**) states that the site historically contained storage tanks or drums for fuel, chemicals or liquid waste.

Resource Management (National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health) Regulations 2011 are considered applicable since the site (i.e. the 'piece of land') is to change its use, soil is to be disturbed/removed (e.g. excavation for foundations) and HAIL activities have been identified on the site. Therefore, based on the above the site is to be assessed under the NES.

A Preliminary Site Investigation (PSI) undertaken by Paddle Delamore Partners (PDP) also identified a number of historical industrial activities. In 2011 PDP undertook soil sampling across the majority of the site excluding locations where existing buildings remained and found concentrations of contaminants (including heavy metals and polycyclic aromatic hydrocarbons (PAHs)) and occasional "hot spots". Whilst the initial PDP soil sampling investigation provides some soil contaminant information, PDP is currently undertaking further soil sampling in order to better characterise the soils. Additionally, PDP have recently undertaken further testing as part of preparing a Detailed Site



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Investigation (DSI) and have identified the presence of asbestos and elevated levels of lead within the site. The results of this DSI will determine the final activity status under the NES, and once the DSI is completed then PDP will lodge a separate resource consent application with both the City Council and the Regional Council to address site contamination issues.

It is acknowledged that, pursuant to Regulations 10(4) and 11 the proposal is a discretionary activity under the NES as the activity is not otherwise provided for as a permitted, controlled, or restricted discretionary activity. Specifically, a Detailed Site Investigation (DSI) of the piece of land has not yet been completed.

Irrespective of the above, and taking a pragmatic approach, it is volunteered by the applicant that a condition of this consent could be to the effect of:

- a) preventing any soil disturbance being undertaken on the site until such time as the DSI has been completed and presented to the Council;
- b) Resource consent in relation to contaminated soils has been approved by the City Council and/or Regional Council as necessary, and;
- b) any disturbance of soil must proceed in accordance with the recommendations of the DSI.

Therefore it is intended to meet the requirements of Section 10(2) prior to earthworks commencing and taking a conservative approach such a restricted discretionary status is considered more appropriate however, it is acknowledged that these conditions are not currently met.

#### 4.8 Other Activities

Not Applicable



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## 5.0 DISTRICT PLAN ASSESSMENT

### 5.1 Status of District Plans

#### Operative Christchurch City Plan

The Christchurch City Plan became operative in part on the 21st November 2005. Rules relating to Section 3 *Central City Business (Core)* zone and rules relating to Section 13 *Transport* within the central city remain operative and are therefore applicable to this development proposal.

While signage rules under this Plan are also still operative, this application does not consider signage as design details are yet to be finalised. As noted earlier, once the details and technical requirements have been finalised if signage should not comply with the relevant City Plan provisions a separate consent will be lodged.

#### Proposed Christchurch Replacement District Plan

Stages 1, 2 and 3 of the Proposed Christchurch Replacement District Plan (pCRDP) have been notified, and the Independent Hearings Panel has made a number of decisions on specific parts of the Plan, including 'Strategic Directions and Strategic Outcomes'. Some of the rules of the pCRDP have legal effect pursuant to section 86B of the Resource Management Act, while others are fully operative.

In relation to this application it is noted that:

- Decisions on Chapter 7 *Transport* (Part) and relevant definitions were notified on the 15<sup>th</sup> August; however these rules and definitions pertain to zones outside the Central City. Therefore, a compliance assessment has been undertaken against Section 13 (Transport) of the operative City Plan as applicable to the central city.
- Decisions on Chapter 8 *Earthworks* (Part) were made operative on the 19th September. An assessment against these provisions has been undertaken by PDP in conjunction with the contamination assessment which forms a separate consent and is being lodged concurrently with this application.

### 5.2 Zoning of Application Site

The site is zoned Central City Business (Core) on Planning Map 39I of the Operative City Plan and is also listed as an existing designation under CER (Justice and Emergency Services Precinct) on Planning Map 39C and 39J.

The proposed Christchurch Replacement District Plan also zones the site Central City Business (Core) on Planning Map CC however Planning Map 39 indicates the designation no longer applies to the site as shown Figure 1 below (application site denoted in yellow). It is imperative to note Council have yet

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to update their public designation map to reflect this change and for the purpose of this application it is considered the designation over the site has been removed in accordance with Figure 2 below.

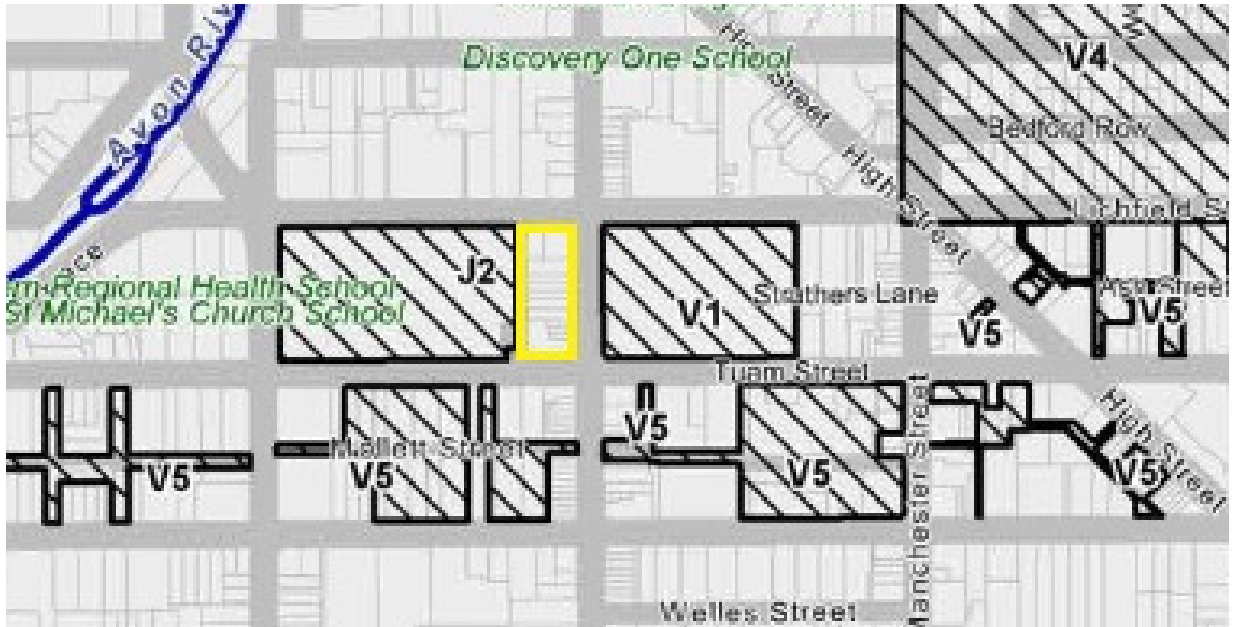


Figure 2: Designation Map showing location of the site with respect to the designation (Extract [http://districtplan.ccc.govt.nz/linkedcontent/planningmaps/PlanningMaps\\_39](http://districtplan.ccc.govt.nz/linkedcontent/planningmaps/PlanningMaps_39))

### 5.3 Operative Plan – Central City Business (Core) Zone Rules

A compliance assessment of the proposal has been undertaken to determine whether the proposal is a permitted activity as specified under Rule 3-2.1.1a for permitted activities. The proposal is for the establishment of an *entertainment exchange* comprising cinemas (place of entertainment) and restaurants as such are permitted activities under 3-2.1.1a.

#### Central City Business Zone Development Standard 3-2.2.1 Building Setback and Continuation

The City Plan specifies buildings shall be built up to road boundaries and across 100% of the site, excluding access ways.

The proposed building is built up along the Colombo Street, Lichfield Street and Tuam Street road boundaries and across 100% of the site except for the main pedestrian entrance at ground floor level in the north-eastern corner of the site. Compliance with this rule is not achieved.

#### Central City Business Zone Development Standard 3-2.2.2 Urban Design

The City Plans specifies that within the core the erection of any new buildings which are visible from a public space shall be a restricted discretionary activity.



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The proposal seeks to erect a new building on the application site and is therefore is to be considered as a restricted discretionary activity.

#### Central City Business Zone Development Standard 3-2.2.5 Active Frontage

For sites denoted in Planning Map 39E requiring active frontages, the use of that part of the ground floor of every building, or any part of a site not occupied by a building, that is within 10m of the boundary of a road (excluding access ways and service lanes) shall be limited to retail activities, commercial services, places of entertainment, reception areas for hotels and travellers' accommodation or pedestrian entranceways for any other activity.

The application site is not required to provide an active frontage in accordance with Planning Map 39E of the City Plan.

#### Central City Business Zone Development Standard 3-2.2.6 Verandas

For sites denoted in Planning Map 39E requiring verandas every building shall provide either a veranda or form of weather protection. The application site is not required to provide verandas in accordance with Planning Map 39E of the City Plan.

#### Central City Business Zone Development Standard 3-2.2.7 Sunlight and Outlook for the Street

Buildings shall not project beyond a recession plane of 45° applying from the maximum road wall height (21m) and angling into the site.

The proposed building is 19m in height and therefore cannot intrude the 45° recession plane as shown in the **Appendix A** elevations. Therefore the proposal complies with this standard.

#### Central City Business Zone Development Standard 3-2.2.8 Minimum Number of Floors

For sites denoted in Planning Map 39I the minimum number of floors above ground level shall be two. The proposed building is three levels (inclusive ground level) as such compliance is achieved.

#### Central City Business Zone Development Standard 3-2.2.9 Minimum Floor to Floor Heights on Ground Floor

There shall be a 4.0m minimum distance between the top of the ground floor surface to the bottom of the first floor surface.

The ground floor of the proposed building has a floor to floor height of 5.04m between the ground and first floor level, thereby achieving compliance.



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Central City Business Zone Development Standard 3-2.2.10 Location of On-Site Parking

Car parking within the Core shall be located to the rear of, on top of, within or under building.

The application site does not provide parking, as such, compliance is achieved.

Central City Business Zone Development Standard 3-2.2.13 Screening of Outdoor Storage and Service Areas/Spaces

Any outdoor storage or service areas/spaces shall be located to the rear of the principal building on the site and shall be screened from any adjoining site by landscaping, fence, wall or a combination of not less than 1.8m high.

An outdoor storage area is located to the rear of the buildings and is screened by the proposed building, ensuring compliance is met.

Central City Business Zone Community Standard 3-2.3.1 Building Height

The maximum height of all buildings shall be 28m. The proposed maximum building height is 19m, thereby achieving compliance.

Central City Business Zone Community Standard 3-2.3.2 Maximum Road Wall Height

Buildings are subject to a 28m height limit and the maximum road wall height shall be 21m.

The proposal has a maximum road wall height of 18m, therefore compliance is achieved.

## 5.4 Operative Plan Compliance Assessment Transport Standards

Development Standard 13-2.4.2 Accessible Parking

The City Plan specifies that other than residential activities, all buildings with a Gross Floor Area of more than 2500m<sup>2</sup> are required to provide parking spaces for people with disabilities, even if no other parking spaces are provided. If no other parking spaces are provided, the amount of disabled parking spaces required shall be calculated by determining how many disabled parking spaces would be required if 1 standard parking space per 100m<sup>2</sup> Gross Floor Area were provided.

Based on a GFA of 10,304m<sup>2</sup> the proposal will require four accessible spaces, whereas none are proposed to be provided.



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#### Development Standard 13-2.4.4 Cycle Parking

For any activity provision shall be made for visitor and staff cycle parking in accordance with Table 6 except that for any activity where the building has no road frontage setback for the entire length of the site (such as this site) visitor cycle parking is not required. Noting if an activity is not specified in Table 6 cycle parking rates in Clause 2.2.1 Table 1 apply.

Table 6 specifies food and beverage activities shall provide 1 visitor space/125m<sup>2</sup> PFA and 1/400m<sup>2</sup> PFA for staff. As mentioned above there is no visitor cycle requirement for buildings which have no road frontage setback, therefore only staff cycle parking is required to be assessed.

In the absence of specific / known tenants for the food and beverage tenancies, it is assumed for the purpose of this application that public floor area (PFA) will be around 67% of 4,548m<sup>2</sup> GFA for the food and beverage tenancies. Based on a PFA of 3,000m<sup>2</sup> (food and beverage) require eight staff cycle parks.

Table 1 in Clause 2.2.1 (Volume 3, Part 13) specifies staff cycle parking provisions for cinemas at a rate of 1 staff cycle park per 50 seats. The seven cinema complex provides a total of 912 seats and therefore requires 18 cycle parks.

Overall, the proposed entertainment exchange is to provide 8 + 18 = 26 cycle parks. The proposal does not provide any cycle parks and therefore does not comply with the City Plan requirements.

#### Development Standard 13-2.4.5 Loading Areas

For all activities in all zones within the Central City minimum loading and unloading requirements shall be provided as per Table 1 in Clause 2.2.1 (Volume 3, Part 13) except where: no on-site parking (other than disabled spaces) is provided, in which case no loading spaces are required.

The proposal does not provide on-site parking and therefore is not required to provide a dedicated loading space. Therefore, compliance with the standard is achieved.

#### Development Standard 13-2.4.12 High Traffic Generators

The City Plans specifies that permitted activities within the Central City are exempt from assessment as High Traffic Generators. However, while the proposed activity is anticipated in the Central City, it is not 'permitted' due to breaching other City Plan standards.

However there are no restricted discretionary assessment matters provided for this standard, and the standard practicably can't be applied. On that basis, it is considered that this standard is *ultra vires* is not applicable to the proposal.



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## 5.5 Replacement District Plan Compliance Assessment - Earthworks

A compliance assessment of the proposal is required to be undertaken to determine whether the proposal is a permitted activity as specified under Rule 8.5A.2.1 for permitted activities.

It is noted that the site is permitted earthworks up to 0.6m in depth and at a ratio of 100m<sup>3</sup> per hectare (equating to 45m<sup>3</sup> for this site). Although final earthworks quantities are yet to be determined, it is highly likely that they will exceed a depth of 0.6m and a volume of 45m<sup>3</sup>. This requires consent as a restricted discretionary activity.

There is also the issue of site contamination. As noted earlier:

- The site is listed on the LLUR as having historically contained storage tanks or drums for fuel, chemicals or liquid waste.
- A PSI undertaken by PDP has confirmed that the site is contaminated.
- A DSI is currently being undertaken by PDP and has identified the presence of asbestos and elevated levels of lead within the site;
- Once the DSI is completed then PDP will lodge a separate resource consent application with both the City Council and the Regional Council to address site contamination issues.
- It is intended to meet the requirements of Section 10(2) prior to earthworks commencing and, taking a conservative approach, such a restricted discretionary status is considered more appropriate.

In relation to the last bullet point, the restricted discretionary status afforded once PDP lodge the separate application dealing with site contamination and earthworks matters does not affect the overall status of this consent application as being for a restricted discretionary activity.

It is important to note that earthworks and site contamination issues will be given no further consideration in this consent application.

## 5.6 Summary of Non-compliances and Activity Status

The preceding compliance assessment identifies the following operative City Plan design non-compliances that will be the subject of this consent application:

### Operative City Plan Non-Compliances:

- Development Standard 3-2.2.1 Building Setback
- Development Standard 3-2.2.2 Urban Design.
- Development Standard 13-2.4.2 Accessible Parking;





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- Development Standard 13-2.4.4 Cycle Parking;

The overall activity status under the operative City Plan is therefore for **restricted discretionary** activity.

It is reiterated that earthworks and site contamination issues considered under the pCRDP will be given no further consideration in this consent application and will instead be the subject of a separate consent application being lodged concurrent with this application.



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## 6.0 ASSESSMENT OF ENVIRONMENTAL EFFECTS

Under Section 104 1) a) of the Resource Management Act (RMA) the consent authority must consider any actual and potential effects on the environment of allowing the activity. Section 104 1) c) extends the extent of discretion further through consideration of any other matter the consent authority considers relevant and reasonably necessary to determine the application.

This application seeks consent for a **restricted discretionary** activity due to non-compliance with three rules of the operative City Plan.

Section 104 C 1) specifies that the consent authority must only consider the matters over which it has discretion. As such, the exercise of the Council's discretion is limited to the assessment matters relevant to the breached rules. It is considered that the assessment matters form a robust measure of any potential effects on the environment and on this basis the following assessment of effects on the environment relevant to this proposal in the order of non-compliances presented in Section 5.6 earlier follows.

### 6.1 Building Setback & Urban Design

It was noted earlier that the northeast corner of the proposed building is not built up to the road boundary. The District Plan specifies that resource consent applications for non-compliance with this rule will only be assessed against the following matters:

- (a) *The extent to which buildings are of sufficient height to enclose the street taking into account the scale of surrounding buildings;*
- (b) *The extent to which buildings are already aligned with the street frontage in the vicinity of the site, and the likelihood of future buildings on sites in the vicinity being aligned with the street frontage if they currently do not contain buildings;*
- (c) *Whether a setback is needed to enable high amenity private open space, and whether this will be integrated with public open space;*
- (d) *The effect on adjacent activities and sites, on utilisation of the street, including by pedestrians, and on the safe and efficient functioning of transport networks in not providing for continuity of building frontage;*
- (e) *The principles of Crime Prevention Through Environmental Design (CPTED).*

In terms of urban design, Development Standard 3-2.2.2 *Urban Design* specifies that the erection of any new buildings within the Core shall be a restricted discretionary activity with due consideration given to the assessment matters provided within the rule. The assessment matters are the extent to which the building or use:



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- (a) *Recognises and reinforces the context of a site, having regard to the identified urban form for the Central City, the grid and diagonal street pattern, cultural elements and public open spaces;*
- (b) *Promotes active engagement with the street, community safety, human scale and visual interest;*
- (c) *Takes account of nearby buildings in respect of the exterior design, materials, architectural form, scale and detailing of the building;*
- (d) *Is designed to emphasise the street corner (if on a corner site);*
- (e) *Is designed to incorporate Crime Prevention Through Environmental Design (CPTED) principles, including encouraging surveillance, effective lighting, management of public areas and boundary demarcation; and*
- (f) *Incorporates landscaping or other means to provide for increased amenity, shade and weather protection.*

In relation to both of the above non-compliances, from the outset it must be noted that the proposal has been subject to consultation with the Council's urban design advisors; Hugh Nicholson and John Lonink prior to consent lodgement. Furthermore, the Applicant presented the proposal before the Urban Design Panel on the 5<sup>th</sup> October 2016. The Urban Design Panel's comments and recommendations were not available at the time of lodgement of this application and will be submitted to Council as an addendum to this application. At this point it is noted that both the Council's urban design advisors and the Urban Design Panel's comments at the presentation were supportive of the proposal.

Turning to the assessment matters above; an architectural design statement has been prepared by Ignite Architects which addresses each of the assessment matters in turn. Rather than reiterate these points the reader is directed to **Appendix C** of the application.

Overall it is considered that the proposal meets the building setback and urban design objectives of the City Plan. No adverse effects are anticipated.

## 6.2 Accessible Parking Provision

The City Plan specifies all buildings with a Gross Floor Area of more than 2,500m<sup>2</sup> are required to provide parking spaces for people with disabilities, even if no other parking spaces are provided. The assessment matters relevant to this standard are contained in Section 13-3.2.16 of the City Plan and are as follows.



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- (a) *Whether the equivalent number of parking spaces, including disabled parking spaces, can be provided on a separate site which is within a suitable distance from the development; and is clearly associated with the development through signage or other means;*
- (b) *Whether the parking can be provided and maintained in a jointly used car parking area;*
- (c) *Whether the required parking can physically be accommodated on the site;*
- (d) *The extent to which the safety of people, including mobility impaired people, will be affected by being set down on-street.*

In relation to above assessment matters the application site is centrally located within the Central City where there are a number of appropriately located public parking facilities within close proximity to the site including, but not limited to, the under-construction Hereford Street parking building, the Crossing and Lichfield Street parking buildings. Each of these buildings are designed with accessible parks within these facilities.

There is an accessible parking space located on street on Tuam Street to the immediate east of the site, and it is further understood Council are to develop on street mobility parks on Colombo Street north of Lichfield Street and Lichfield Street west of Colombo Street. It is anticipated that these accessible parks, in conjunction with those located within nearby public parking facilities, will provide sufficient accessible parking spaces to meet the demand of visitors.

Each of the public facilities provide safe pedestrian crossing routes, coupled with pedestrian crossings of roads within the core controlled via signalised crossings, thereby ensuring the safety of users.

It is therefore considered that only very minor adverse effects will arise from this identified non-compliance.

### 6.3 Cycle Parking Provision

Based on a provision of a seven cinema complex providing a total of 912 seats and a PFA of 3,000m<sup>2</sup> (food and beverage) the City Plan cycle parking requirement is proposal require 26 staff cycle parking spaces. The proposal has a nil cycle park provision. The assessment matters relevant to this standard are contained in 13-3.2.17 of the City Plan as follows.

- (a) *The extent to which alternative adequate cycle parking is available which is within easy walking distance of the development entrance;*
- (b) *Whether the parking can be provided and maintained in a jointly used cycle parking area;*
- (c) *The extent to which cycle parking facilities are designed and located to match the needs of the intended users.*
- (d) *Whether the provision for cyclists is sufficient considering the nature of the activity on the site and the anticipated demand for cycling to the site and adjacent activities;*
- (e) *Whether the provision for cyclists is practicable and adequate considering the layout of site, and the operational requirements for the activity on the site.*



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As mentioned previously, the application site is located directly opposite the central bus exchange which provides a large covered secure cycle park facility (87 indoor spaces plus 20-30 outdoor spaces). This existing high quality parking facility which is located only 20 metres from the site and is currently significantly underutilised. The bus exchange cycle parking facility will adequately cater for the anticipated demand generated by the proposed activity in conjunction with other central city activities and will adequately meet the needs of the intended users in that the spaces are covered, secure and/or conveniently located. It is considered that no adverse effects will arise from this identified non-compliance.

#### 6.4 Effects Summary

For the reasons outlined above, it is considered that any adverse effects on the existing environment will be less than minor, and no persons are considered adversely affected by the proposal.

#### 6.5 Consideration of Alternatives

It is considered any adverse effects of the proposed development on the environment relate solely to the identified non-compliances discussed above. As concluded above the effects associated with the proposal will be less than minor. Accordingly, under Schedule 4 clause 6(1)(b) of the Resource Management Act, there is no need for alternatives to be considered.



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## 7.0 ASSESSMENT OF OBJECTIVES AND POLICIES

### 7.1 Status of Operative Christchurch City Plan and Replacement District Plan

This application seeks consent for a restricted discretionary activity due to non-compliance with four standards. The preceding sections of this application have considered the relevant rules of the City Plan and the replacement District Plan and the relevant assessment matters in association with the effects on the environment.

This section of the application assess the proposal in relation to the relevant objectives and policies of the City Plan and the Proposed District Plan.

The relevant operative City Plan objectives and policies are contained within Section 7 *Transport* and 12 *Business*.

Stages 1, 2 and 3 of the proposed Christchurch Replacement District Plan (pCRDP) have been notified, and the Independent Hearings Panel has made a number of decisions on specific parts of the plan. While some rules may not yet have legal effect, regard must be had to relevant objectives and policies and other provisions. The relevant objectives and policies are found within Chapter 3 Strategic Directions contains a number of objectives which guide the recovery and future development of the City, these objectives and policies generally reinforce the operative City Plan provisions and therefore further assessment is not made. Instead this application considers the relevant objectives and policies of Chapter 13 Central City of the pCRDP.

### 7.2 Operative Plan Section 7: Transport

#### Objective 7.9 Central City Transport Objective

*“An accessible Central City for all people no matter how they choose to travel.”*

The proposal is considered consistent with Objective 7.9 and its supporting policies in that the development is within close proximity to an adequate amount of good quality (secure, covered and convenient) on-site cycle parking and also presents a site layout that provides safe and convenient pedestrian routes to/from the site (Policy 7.9.2). The site is located opposite the central bus exchange and therefore is well served by public transportation (Policy 7.9.3).



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## 7.3 Operative Plan Section 12: Business

### Objective 12.2 Role of the Central City

*“To restore and enhance the Central City as Christchurch's principal focus for a diversity of business, accommodation, community and cultural activities, while managing adverse effects arising from the intended mix of activities”.*

The proposal is considered consistent with Objective 12.2 and its supporting policies. In particular, the proposal seeks to establish an *entertainment exchange* within the central city core. The building is of a significant scale comprising three levels and bounded by three road frontages consistent with Policy 12.2.1. The activity itself will be a vibrant addition to the city centre by providing a range of entertainment experiences for workers, visitors and residents alike. (Policy 12.2.2). The building is of an exemplar design with many architectural design elements and themes inspired by the Canterbury region such as the Canterbury Plains and Southern Alps along with materials being sourced from local sustainable suppliers thereby supporting policy 12.2.3.

The building complies with bulk and location standards of the City Plan and has been considered by Councils principal urban designer ensuring there will be no effects on the function and amenity of the central city.

### Objective 12.3 Efficiency and Viability

*“To enable the efficient use and continued viability of the physical resources of the Central City, and promote the economic success and vitality of the area.”*

The proposal is considered consistent with Objective 7.9 and its supporting policies as it seeks to develop an undeveloped site within central city site for the establishment of a permitted activity which is considered compatible with activities which can establish within the central city. The building has been designed to provide not only active frontages yet also provides an identifiable entrance which links to the footpath interface thus providing a connection to the streetscape. As mentioned previously, the proposal complies with the bulk and location standards therefore ensuring adaptability of the site and form for future use.

For the reasons above, it is considered that the proposal is in accordance with the District Plan transport and business objectives and policies.

## 7.4 Replacement District Plan Chapter 13 Central City

The relevant objectives and policies are found within Chapter 13 Central City contains a number of objectives and policies which reinforce the operative City Plan provisions discussed earlier and will not be revisited.



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Overall it is considered that the proposal is entirely consistent with City Plan Objective 12.2 and 12.3 and the relevant supporting policies.

## 7.5 Conclusion on the Objectives and Policies Assessment

As decisions on the Central City chapter of the pCRDP have not yet been released, it is considered that greater weight should be given to the corresponding provisions in the Operative Plan. Nonetheless, it is ultimately concluded that the proposal is not contrary to the relevant objectives and policies of the City Plan or the pCRDP.

## 8.0 STATUTORY EVALUATIONS

### 8.1 Recovery Strategy

The Recovery Strategy for Greater Christchurch (the *Recovery Strategy*) prepared by CERA under the Canterbury Earthquake Recovery Act became operative on 1 June 2012. It is a statutory document that must be "*read together with, and forms part of*" other relevant legislation within the greater Christchurch area. The City and District Plans (and a number of other statutory documents) must not be interpreted or applied in a way that is inconsistent with the Recovery Strategy. Only Section 3-8 of the Strategy has statutory effect.

"*Recovery*" is defined under the CER Act as including both restoration and enhancement (Section 3).

Section 4 identifies the vision for the recovery of Greater Christchurch and supporting goals relating to the six components of recovery. The following goals are of particular relevance to this application:

**Leadership and Integration** – Coordination between public and private sector, and communities to contribute to recovery and future growth by:

- Facilitating a timely and efficient recovery

**Economic** – Revitalise greater Christchurch by:

- Restoring the confidence of the business sector to enable economic recovery and growth
- Identifying and facilitating increased opportunities for early and substantial local and international investment;
- Ensuring a range of employment options

**Built Environment** – Develop resilient, cost effective, accessible and integrated infrastructure, building, housing and transport networks by:





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- Prioritising infrastructure investment that contributes during recovery and into the future
- Rebuild infrastructure and buildings in a resilient, cost-effective and energy-efficient manner.

Section 5 of the Recovery Strategy identifies a number of priorities for recovery to address and promote social, economic, cultural and environmental wellbeing. These include:

- People's safety and wellbeing
- Supply of land for recovery needs

The proposal seeks to establish an “*entertainment exchange*” within the Central City which will offer visitors and residents an entertainment experience not currently on offer. The proposed activities will assist in creating a vibrant city with diverse services of offer which will assist in facilitating growth and investment. It is considered that the proposal is not inconsistent with the Recovery Strategy promotes the identified goals or priorities for recovery.

## 8.2 Land Use Recovery Plan

The Land Use Recovery Plan (LURP) sets a policy and planning framework necessary to:

- Rebuild existing communities;
- Develop new communities;
- Meet the land use needs of businesses;
- Rebuild and develop the infrastructure needed to support these activities;
- Take account of natural hazards and environmental constraints that may affect rebuilding and recovery.

The LURP specifies a planning framework to meet the needs of business. The proposal seeks to establish within a zone that enables the operation to function efficiently without unduly compromising the environment.

The proposal will support the growth and regeneration of the central city by creating employment opportunities for those staff employed in the activity, and most importantly, the proposed activity will provide a entertainment experience which meets the needs of the local and wider community yet also visitors.

Section 23 of the Canterbury Earthquake Recovery Act 2011 states that Council's must not act inconsistently with the Land Use Recovery Plan. As stated above the proposal is not considered to be contrary to the Land Use Recovery Plan and as such, satisfies the requirements of Section 23 of the CER Act.



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### 8.3 Regional Policy Statement

The proposal does not raise any issues of significance that are relevant to the Canterbury Regional Policy Statement.

### 8.4 National Environmental Standard (NES)

As mentioned previously, the proposal has been assessed against the provisions of the NES. As the site (i.e. the 'piece of land') is to change its use, soil is to be disturbed/removed (e.g. excavation for foundations) and HAIL activities have been identified on the site. Therefore, based on the above the site is to be assessed under the NES. An assessment against these provisions is being undertaken by PDP and will form a separate consent.

### 8.5 Resource Management Act

In considering this application the purpose of the Resource Management Act should be taken as the primary point of reference. Pre-eminence must be given to Part II, the purpose and principles of the Act.

#### Part II - Section 5

The purpose of this Act is to promote the sustainable management of natural and physical resources. Section 5 imposes a duty on consent authorities to promote "*sustainable management*" while avoiding, remedying, or mitigating any adverse effects of activities on the environment.

In regards to Section 5, the proposal is consistent with the enabling component of the Act in that the proposal will be providing for the efficient and sustainable use of the site, without compromising the quality of the surrounding environment.

#### Part II - Section 6

It is not considered that any part of Section 6 '*Matters of National Importance*' are relevant in this case.

#### Part II - Section 7

Particular regard should also be given to Section 7 "Other Matters":

*(b) The efficient use and development of natural and physical resources:*

*(c) The maintenance and enhancement of amenity values:*

*(f) Maintenance and enhancement of the quality of the environment:*

The proposal seeks to develop a site that has been zoned specifically for places of entertainment and food and beverage activities. It is considered that this development is an efficient and effective use of



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physical resources. The proposal seeks to develop the site to a high level of amenity with exemplar building design featuring materials and design elements inspired by the Canterbury region. Overall it is considered that this proposal is in consistent with the intentions of Part II of the Act.

#### Part II - Section 8

Similarly there are no known matters that are considered relevant to the Treaty of Waitangi (Section 8) which need to be taken into account.

#### Part II - Conclusion

Overall it is considered that this proposal is in consistent with Part II of the Act.

#### Sections 104 A- D

Section 104 sets out those matters that must be considered when assessing an application for a resource consent. Subject to Part 2 of the Act, Section 104(1) requires a consent authority to have regard to the following matters:

- 104 (1) a) any actual and potential effects on the environment of allowing the activity*
- 104 (1) b) any relevant objectives, policies, or rules of a plan or proposed plan, regional policy statement or proposed regional policy statement, or national environmental standard (NES) or national policy statement (NPS)*
- 104 (1) c) any other matter considered relevant and reasonably necessary to determine the application.*

As outlined in the preceding assessment of effects, it has been considered that the effects on the environment are less than minor and therefore the test under s104(a) has been met.

It is considered that the application is consistent with the objectives and policies of the Christchurch City Plan and Proposed District Plan and can be granted with appropriate conditions imposed under section 108 of the Act.

An assessment of other relevant statutory documents is provided in Section 7 of this application.

Accordingly it is considered that the application may be granted under Section 104C (2) of the Act.

## 9.0 MONITORING

As there are no significant adverse effects on the environment, no on-going monitoring of the proposal is required or proposed.



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## 10.0 CONSULTATION

Clause 6(1)(f) of Schedule 4 to the Resource Management Act states that an Application should include "Identification of the persons affected by the activity, any consultation undertaken, and any response to the views of any person consulted".

The effects arising from the proposal have been considered in Section 5 of this application and are considered to be less than minor. Given that the effects on the proposal are less than minor we do not consider there are any adversely affected parties.

This proposal has been subject to extensive consultation with Hugh Nicholson, Council's Principal Urban Design officer and John Lonink as well as Council traffic engineer Mike Calvert. The proposal was presented to the Urban Design Panel on the 5<sup>th</sup> November 2016.

## 11.0 NOTIFICATION

Under Section 95A(2), a consent authority must publicly notify the application if it decides (under Section 95D) that the activity will have or is likely to have adverse effects on the environment that are more than minor.

The potential effects arising from the proposal have been assessed in Section 6 of this application and it is considered that the proposal will have, less than minor adverse effects.

It is therefore not considered necessary to notify this proposal to any other person as the effects of the proposal are contained within the development area. There are no affected persons under Section 95E.

Additionally, there are no special circumstances which exist that would require notification under Section 95A(4).



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## 12.0 CONCLUSION

This application relates to the establishment of a three level 10,304m<sup>2</sup> entertainment exchange comprising a 912 seta / seven screen cinemplex with associated food and beverage areas within the Central City Business zone at 617-649 Colombo Street.

The development proposal has been assessed against the relevant rules of the City Plan and design compliance is achieved apart from four matters relating to:

- Building setback;
- Urban Design
- Provision of accessible parking; and
- Provision of cycle parks;

The overall activity status is for a restricted discretionary activity. This activity status restricts the exercise of the consent authority discretion to the assessment matters relevant to the breached rules.

In regards to building setback and urban design the proposal has been subject to consultation with Councils urban design advisors and is to be presented to the Urban Design Panel. Nonetheless the building is of an exemplar design with many architectural design elements and themes inspired by the Canterbury region. The building entrances and active frontages connect with the streetscape which creates a vibrant and inviting experience.

The provision of accessible parking spaces within public facilities and on street coupled with safe and convenient access routes are considered more than adequate to meet the demand for accessible users.

Despite the nil cycle parking provision it is considered that public facilities within proximity to the application site will more than cater for the expected demand.

Overall, it is considered the proposal will be consistent with the scale and intensity of development anticipated in the zone. It has been demonstrated by the preceding assessment that any adverse effects of the on the environment are less than minor.

The proposal is consistent with the Objectives and Policies of the proposed District as outlined in Section 7. The relevant provisions of Part II of the Resource Management Act 1991 have been satisfied and it is considered that the proposed development is consistent with the purpose of the Act with less than minor environmental effects.

In accordance with sections 95, 95A-95F, 104, and 104C of the Act, consent may be granted.



# DESIGN STATEMENT

## Hoyts Entertainment Exchange



### INTRODUCTION

The Entertainment Exchange is conceived as an integrated entertainment and dining development that connects to the surrounding streetscapes and provides a social hub within the CBD.

**The extent to which the building or use:**

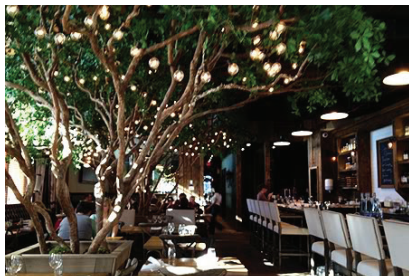
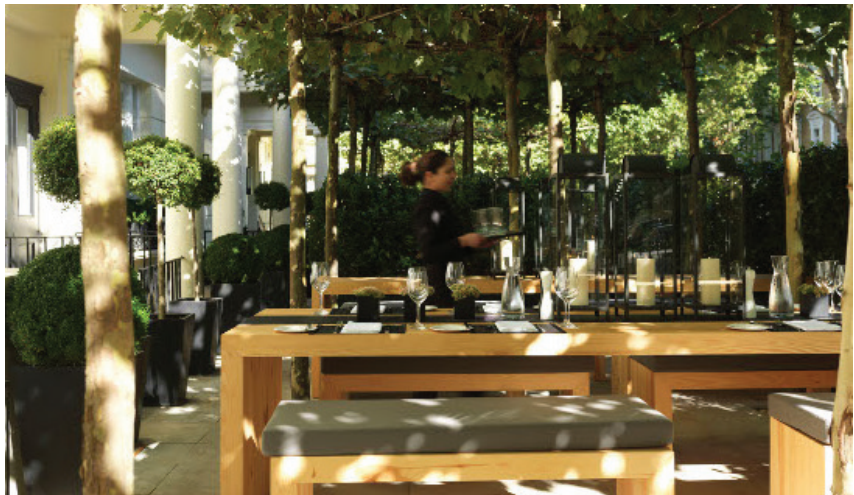
**(a) Recognises and reinforces the context of a site, having regard to the identified urban form for the Central City, the grid and diagonal street pattern, cultural elements and public open spaces;**

The proposed cinema complex bookends the eastern boundary of the Justice Precinct. Facing the Bus interchange to the east on Colombo Street. The Ballantynes corner of the Retail Precinct to the north on Lichfield Street & “the Frame” on Tuam Street.

To the west abutting the proposal is the Justice department which is 4 levels with a parapet of around 20.5m above grade. To the north -Ballantynes is a 3 storey with a circa 14m high parapet. While the bus interchange to the east is a single story building with an extremely articulated roof form circa 8m at ridge line. It is not known to the author what is proposed for the Tuam St. properties opposite the site that form part of “The Frame” However we assume it will be multi-story residential in nature.

The proposed cinema complex parapet varies on all 3 Street. edges between 19 & 17.5m –or mid way between that of the Justice and Ballantynes parapets.

On the columbo street. elevation-opposite the bus interchange an undulating veranda with the circular columns is proposed echoing at a



similar height and with similar language the bus interchange roofline.

**(b) Promotes active engagement with the street, community safety, human scale and visual interest;**

**Active Engagement with Street.**

The ground floor dining precinct presents itself towards retail patrons circulating to the rear of Ballantynes and shoppers coming and going to the bus station.

The pavement interface at Lichfield, Colombo and Tuam Street is envisaged as being open, with glazed restaurant frontages. During opening hours the entire ground floor of the building will be bustling with activity with

direct visual contact to city residents. Opportunities to create openings in the tenancy facades will enable further activation of the streetscape during summer months.

**Human Scale & Visual Interest**

On columbo Street, the undulating veranda roof between 3 & 5m provides visual interest and frames the human scale.

On Tuam Street retail tenancies are proposed of a single story with glass veranda over, with steps in the façade articulating the differing masses of the concrete cinemas from the glass atrium and dining complex and the green “box hedge” to Columbo st.

The green box hedge terminates on the corner of Columbo & Lichfield Street with a wrap around LCD screen facing the retail precinct & bus station. The parapet drops down and slightly back along Lichfield Street accentuating the corner. & providing architectural drama. This is counterpointed at ground level and with a more human scale with two food and beverage



# DESIGN STATEMENT

## Hoyts Entertainment Exchange

tenancies on ground and first floor tucked under the mass of the cinema. Whilst the triple height glass façade illuminates the streetscape & draws attention to the internal activities.

**(c) Takes account of nearby buildings in respect of the exterior design, materials, architectural form, scale and detailing of the building;**

Columbo St. Bus interchange, single level highly articulated roof. Calder Stuart standing seam roof, white circular columns vertical T&G timber cladding & glass.

Proposal is for glass, T&G timber soffit to highly articulated canopy, Calder Stuart standing seam cladding- green patina.

Tuam St. nearby building materials and design unknown

Lichfield St. Ballantynes Composite Aluminium cladding with flat façade, glazing strips continuous parapet.

Proposal to this streetscape is triple height glass concrete and Calder Stuart Standing seam copper patina cladding.

**(d) Is designed to emphasise the street corner (if on a corner site);**

The main entrance addresses the corner of Colombo and Lichfield St with a triple height atrium glass façade clamped between the anchored concrete box of the cinema's and the undulating "green box hedge" echoing the wind breaks of the Canterbury plains. The green box hedge steps up to the corner with wrap around LED screen and the glazed atrium illuminating like a light house the corner- drawing in pedestrians as they move out from the retail precinct.

**(e) Is designed to incorporate Crime Prevention Through Environmental Design (CPTED) principles, including encouraging surveillance, effective lighting, management of public areas and boundary demarcation; and**

All street frontages of this new complex create a visual connection to the activity both within the complex and on the street. During opening hours there will be a passive surveillance implicit

from both the patrons of the complex as much as by the staff on site.

After hours, background lighting from within the complex and street lighting mounted in the street canopies will provide a well-lit pavement zone that will complement the council's ambition to consolidate the bus station as a safe environment for travellers in the city at all hours.

Boundary demarcation is clear with very little in the way of recesses that can not be passively observed.

**(f) Incorporates street frontages of this new complex landscaping or other means to provide for increased amenity, shade and weather protection.**

Shade and weather protection is provided by veranda roofs on all elevations.

Landscaping will be designed at the street frontages to enhance wind breaks along the Colombo St elevation, in order to maximise opportunities for activities during the warmer months of the year.

**Location**

The strategic location opposite the bus and transport exchange on Colombo St, Ballantynes and the new 'Crossing' Retail Centre will provide a key entertainment and dining destination for Christchurch.

**Dining F+B**

The dining precinct will address the street on all building faces to Lichfield, Colombo and Tuam Streets providing permeability and activation to the street frontages. Internally the development will provide a range of food and dining experiences from the more intimate and smaller food offers to larger restaurant based concepts. This dining experience will be tailored to the variety of spatial opportunities presented by the structure of the building

The dining and food environment floats through the base of the development with cinema spaces stacked above and main entry with a grand multi-height space on the corner of Lichfield

and Colombo Street. This provides maximum exposure to sunlight on the northern edge during the day and provides a welcoming beacon towards the city after nightfall.

F+B concepts will embrace the natural/ organic and link to local ranching, food harvesting and trading history.

The F+B concourse will incorporate references to the nearby Avon River and deliver a natural feel of the Canterbury countryside starting with the river's route through the city and back out to the wider farmlands that have helped shape the most enduring images of the region.

**Architectural Concept**

The building is composed of 3 forms, signified with material and shape. The Dining F&B concourse- is a glass box is sandwiched between the heavy concrete anchor of the cinema boxes and the "green box hedge" running the length of Columbo St. this green box hedge evoking the wind breaks throughout the Canterbury plains.

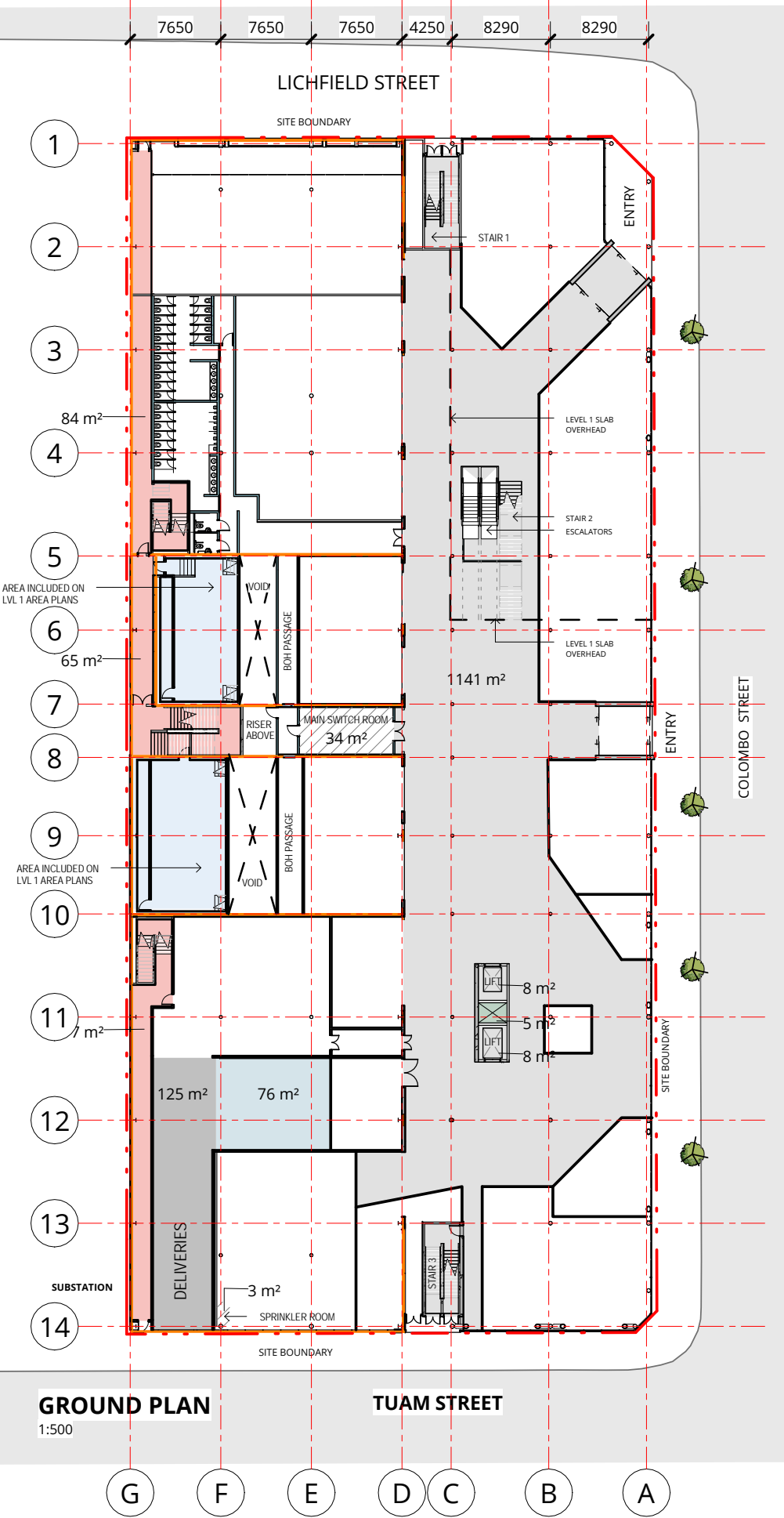
A 4th secondary formal element the veranda on Columbo Street evokes the outline of the Southern Alps and echoes the form of the bus exchange across the road.

This variegated Southern Alps form is repeated in the ceiling above the dining F& B zone While the variegated branching of the columns is intended to evoke an experience of dining beneath the tree canopy.

**Materials**

Inspiration for cladding colour, structure, form and texture is derived from the various themes throughout the Canterbury countryside – box hedges, field patterns and mountain ranges- and material selections are influenced by these connections.

Materials will be selected predominantly from local sustainable sources providing elements that are low maintenance, environmentally sensitive and ultimately timeless through local and historical connection.



PRELIMINARY HOYTS LEASE AREAS (METHOD 3)	
Name	Area
GROUND FLOOR	
HOYTS (BOH)	76 m <sup>2</sup>
HOYTS (EGRESS)	227 m <sup>2</sup>
FIRST FLOOR	
HOYTS	2914 m <sup>2</sup>
HOYTS (ADDITIONAL BOH)	13 m <sup>2</sup>
HOYTS (ADMIN)	131 m <sup>2</sup>
HOYTS (EGRESS)	42 m <sup>2</sup>
HOYTS (WC)	106 m <sup>2</sup>
SECOND FLOOR	
HOYTS	2462 m <sup>2</sup>
HOYTS (EGRESS)	45 m <sup>2</sup>
HOYTS (WC)	39 m <sup>2</sup>
	6056 m <sup>2</sup>

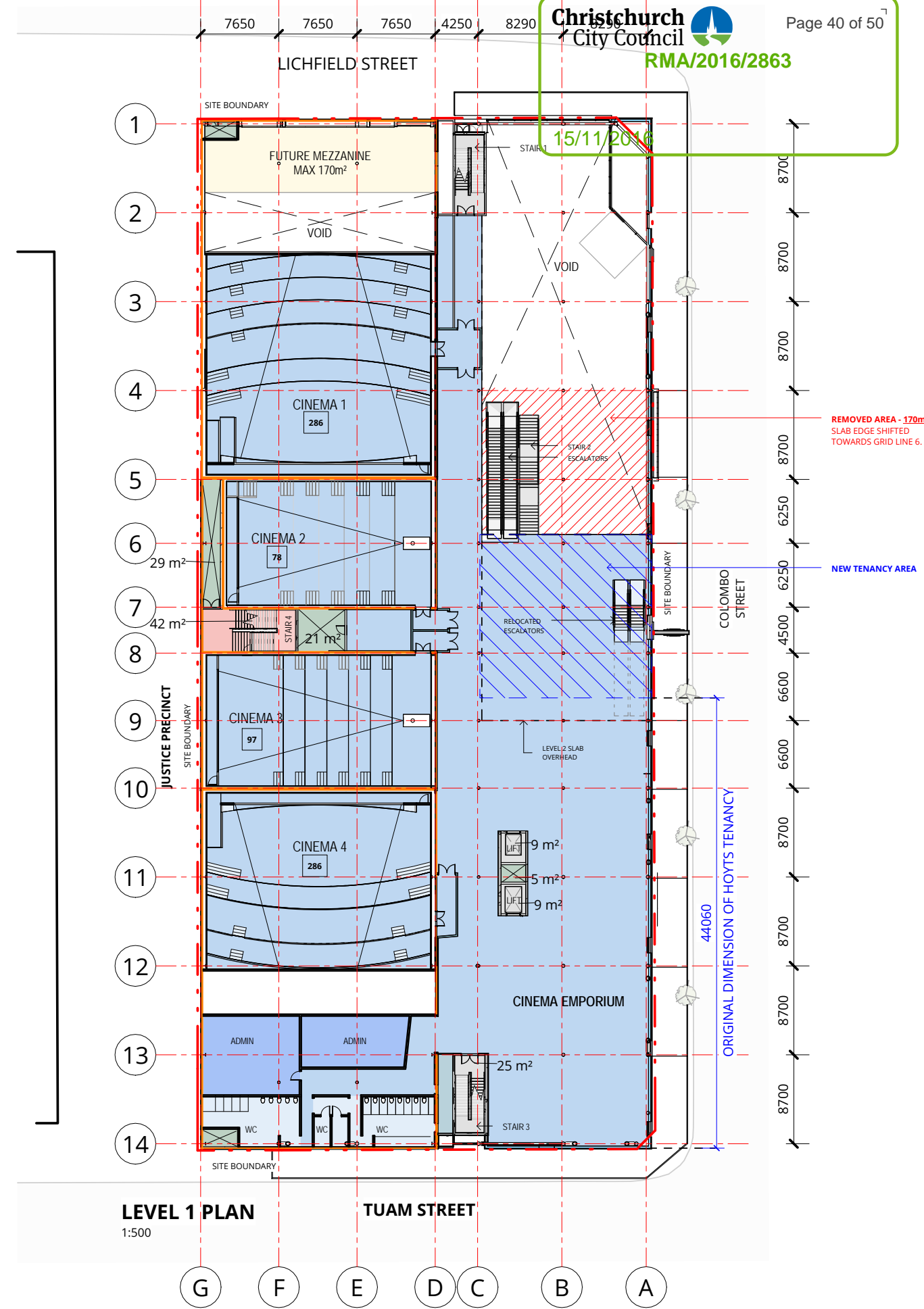
PREVIOUS HOYTS TENANCY AREA 5756m<sup>2</sup>  
HOYTS TENANCY INCREASED BY 310m<sup>2</sup>

PRELIMINARY SHARED LEASE AREAS (METHOD 3)	
Name	Area
GROUND FLOOR	
SHARED (BOH SERVICES)	37 m <sup>2</sup>
SHARED (BOH)	125 m <sup>2</sup>
SHARED (CIRCULATION)	1207 m <sup>2</sup>
SHARED (RISER)	5 m <sup>2</sup>
FIRST FLOOR	
SHARED (CIRCULATION)	135 m <sup>2</sup>
SHARED (RISER)	69 m <sup>2</sup>
SECOND FLOOR	
SHARED (RISER)	57 m <sup>2</sup>
	1636 m <sup>2</sup>

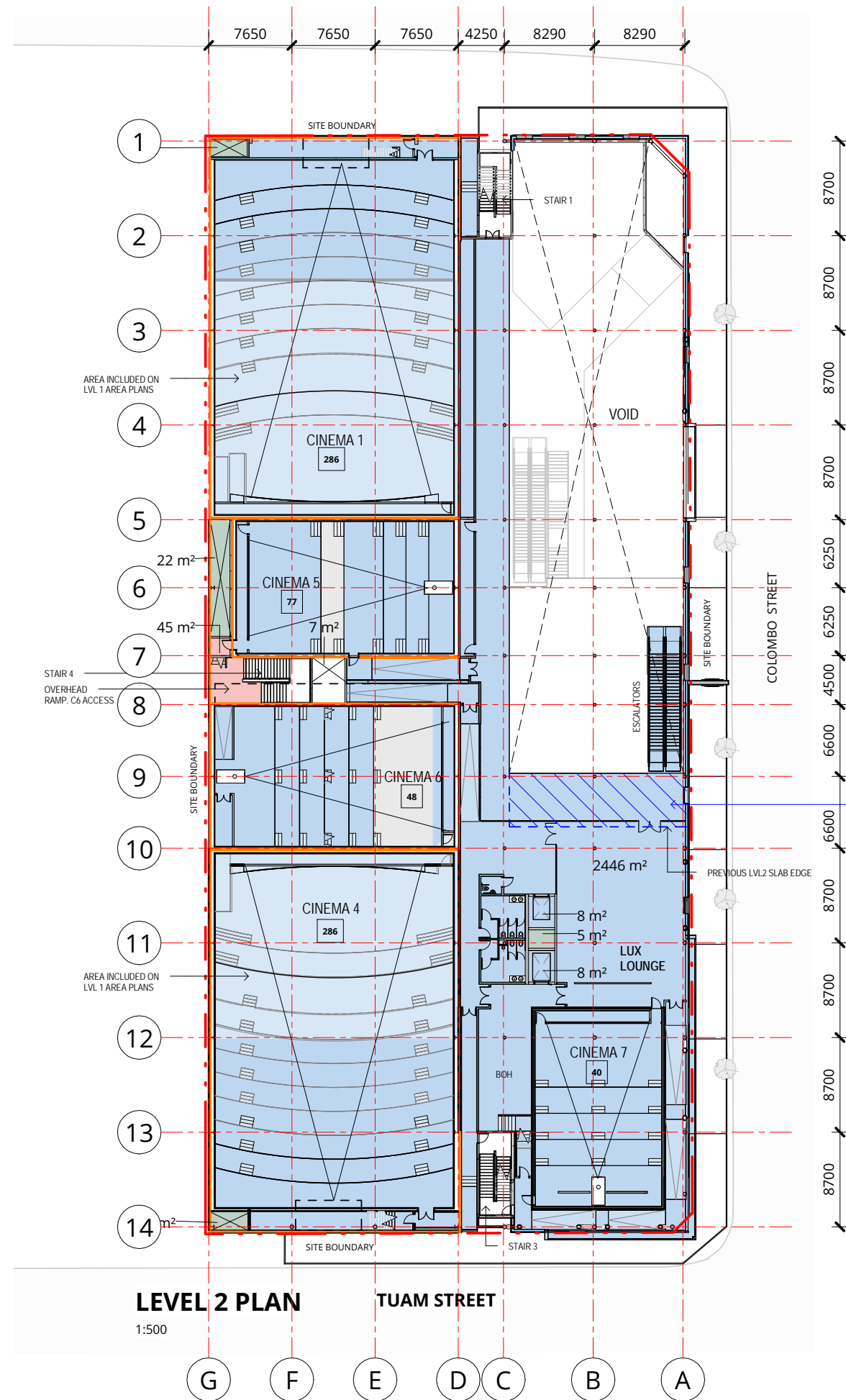
**PRELIMINARY AREAS ONLY**

CONCEPT DESIGN PLANS ONLY.  
SUBJECT TO CHANGES FROM TENANCY  
DISCUSSIONS & STRUCTURAL CO-  
ORDINATION.

PRELIMINARY LEASE AREAS	
	FUTURE MEZZANINE
	HOYTS
	HOYTS (ADDITIONAL BOH)
	HOYTS (ADMIN)
	HOYTS (BOH)
	HOYTS (EGRESS)
	HOYTS (WC)
	SHARED (BOH SERVICES)
	SHARED (BOH)
	SHARED (CIRCULATION)
	SHARED (RISER)
	SIZED AND LOCKED CINEMA BOXES 3/08/2016







**LEVEL 2 PLAN**  
 1:500  
**TUAM STREET**

**PRELIMINARY HOYTS LEASE AREAS (METHOD 3)**  
 15/11/2016

Name	Area
<b>GROUND FLOOR</b>	
HOYTS (BOH)	76 m <sup>2</sup>
HOYTS (EGRESS)	227 m <sup>2</sup>
<b>FIRST FLOOR</b>	
HOYTS	2914 m <sup>2</sup>
HOYTS (ADDITIONAL BOH)	13 m <sup>2</sup>
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HOYTS	2462 m <sup>2</sup>
HOYTS (EGRESS)	45 m <sup>2</sup>
HOYTS (WC)	39 m <sup>2</sup>
<b>Total</b>	<b>6056 m<sup>2</sup></b>

PREVIOUS HOYTS TENANCY AREA 5756m<sup>2</sup>  
 HOYTS TENANCY INCREASED BY 310m<sup>2</sup>

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<b>SECOND FLOOR</b>	
SHARED (RISER)	57 m <sup>2</sup>
<b>Total</b>	<b>1636 m<sup>2</sup></b>

**PRELIMINARY AREAS ONLY**  
 CONCEPT DESIGN PLANS ONLY. SUBJECT TO CHANGES FROM TENANCY DISCUSSIONS & STRUCTURAL CO-ORDINATION.

- PRELIMINARY LEASE AREAS**
- F&B - STAGE 1
  - F&B - STAGE 2
  - F&B COMMON SERVICES AREAS
  - HOYTS
  - HOYTS (ADDITIONAL BOH)
  - HOYTS (ADMIN)
  - HOYTS (BOH)
  - HOYTS (CIRCULATION)
  - HOYTS (EGRESS)
  - HOYTS (WC)
  - SHARED (BOH SERVICES)
  - SHARED (BOH)
  - SHARED (CIRCULATION)
  - SHARED (RISER)
  - FUTURE MEZZANINE

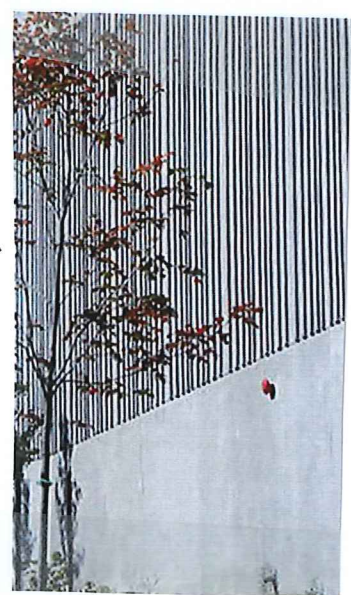
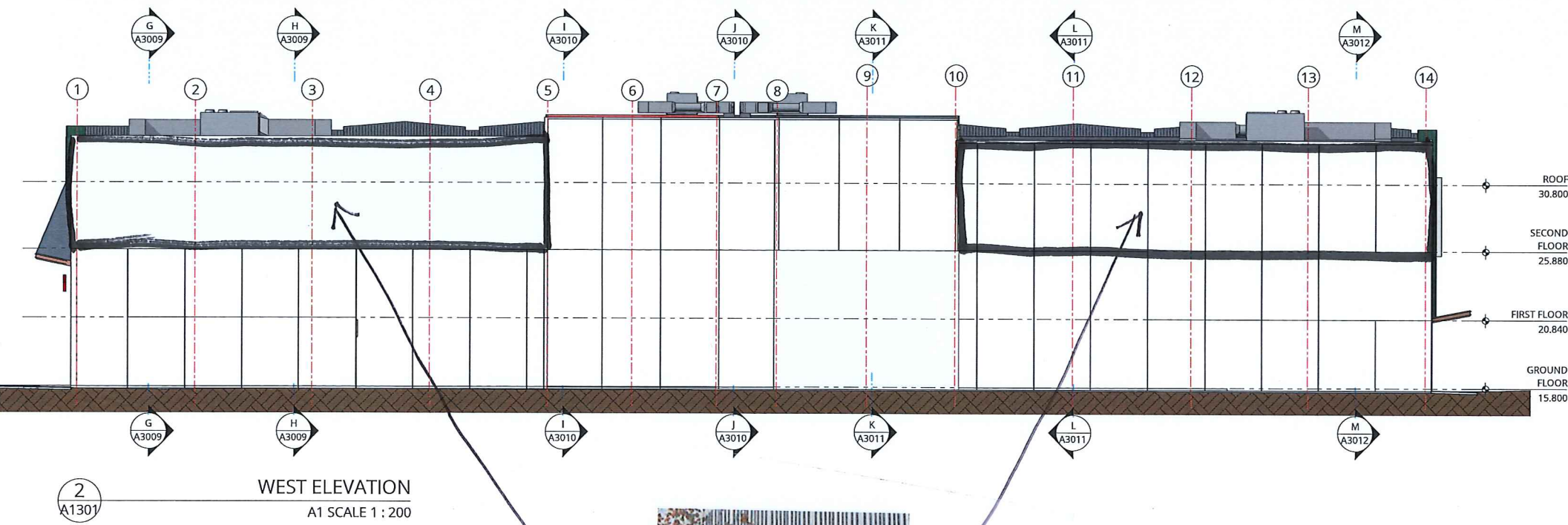
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Rev	Description	By	Date
A	STAGE 1 CONSENT	IA	15/11/16

**UNCONTROLLED**  
**DEVELOPED DESIGN**  
 CONSULTANTS



PROPOSED  
 REBATE TO  
 'ZONES' ON  
 WEST ELEVATION

CONSULTANTS

Calder Stewart  
 Development

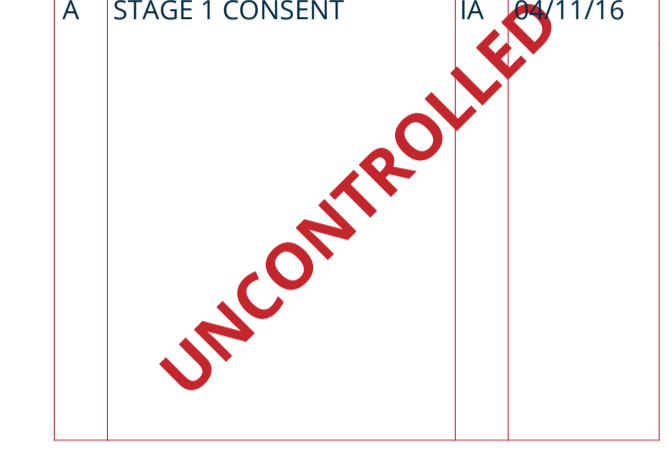
**HOYTS**

PROJECT NAME  
 HOYTS EntX  
 617-649 COLOMBO STREET,  
 CHRISTCHURCH  
 Drawing Title  
 EAST & WEST ELEVATIONS

SCALE 1:200 @ A1

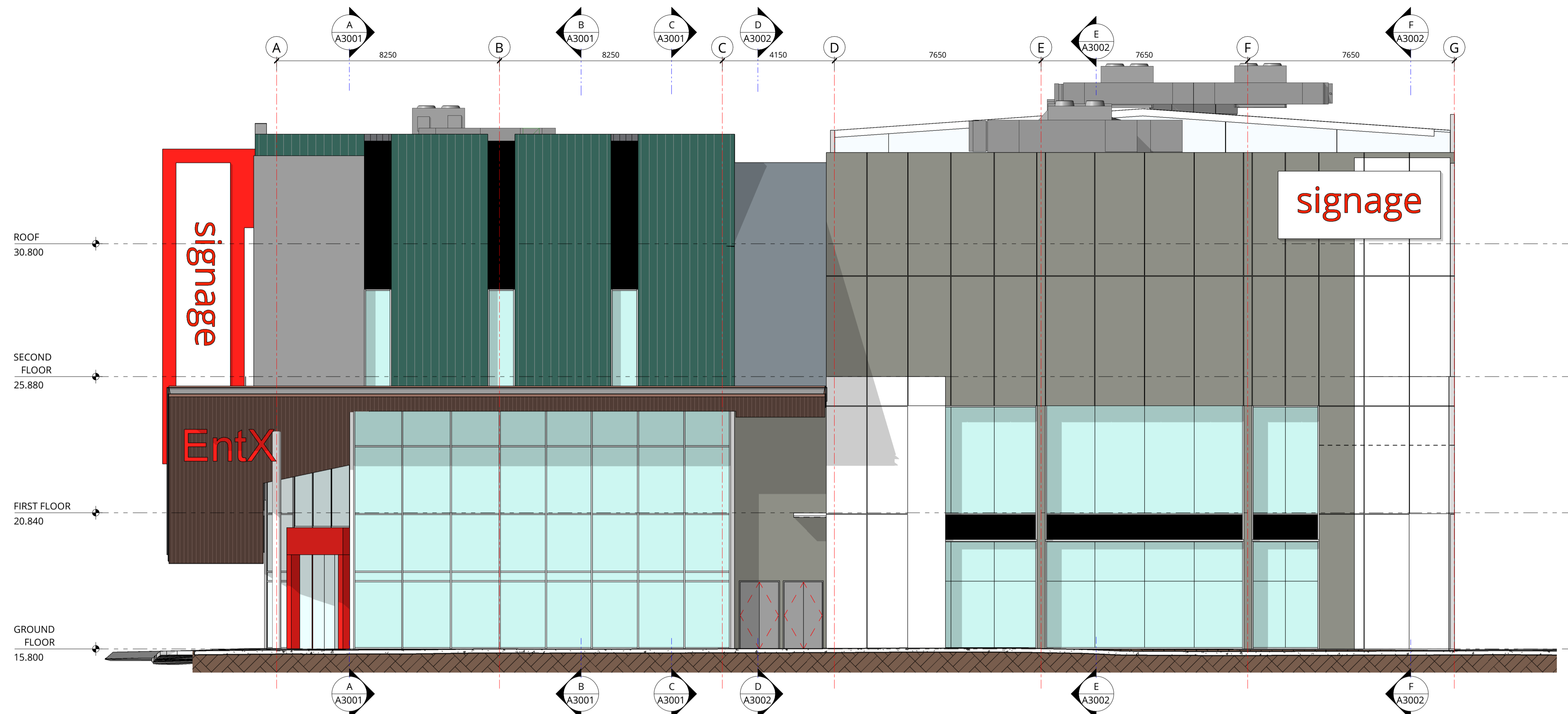
Architect	Drawn	Checked
IGNITE	IGNITE	IGNITE
Project Commenced		Size

Project Number	Drawing Number	Rev
0281-002	A2002	A

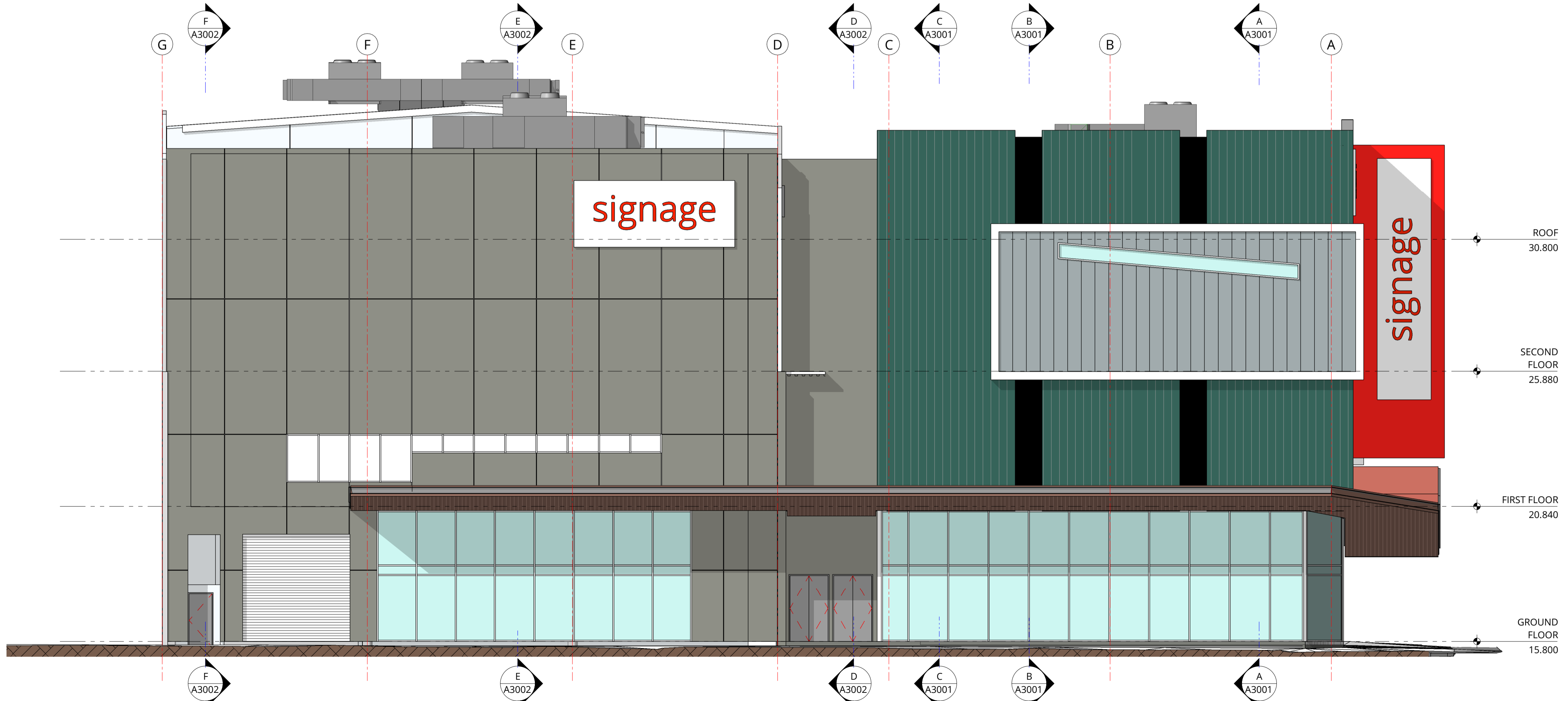


**DEVELOPED DESIGN**

CONSULTANTS



1  
 A1301  
**NORTH ELEVATION**  
 A1 SCALE 1 : 100



2  
 A1301  
**SOUTH ELEVATION**  
 A1 SCALE 1 : 100

CONSULTANTS

Calder **Stewart**  
 Development

**HOYTS**

PROJECT NAME  
**HOYTS EntX**  
 617-649 COLOMBO STREET,  
 CHRISTCHURCH  
 Drawing Title  
**NORTH & SOUTH ELEVATIONS**

SCALE 1 : 100 @ A1

Architect	Drawn Author	Checked Checker

Project Number	Drawing Number	Rev
0281-002	A2001	A

UNIT 1 - 260sqm

UNIT 4 - 145sqm

UNIT 7 - 25sqm

UNIT 10 - 145sqm

UNIT 13 - 45sqm

UNIT 16 - 105sqm

UNIT 2 - 180sqm

UNIT 5 - 130sqm

UNIT 8 - 45sqm

UNIT 11 - 150sqm

UNIT 14 - 55sqm

UNIT 17 - 145sqm

UNIT 3 - 130sqm

UNIT 6 - 150sqm

UNIT 9 - 50sqm

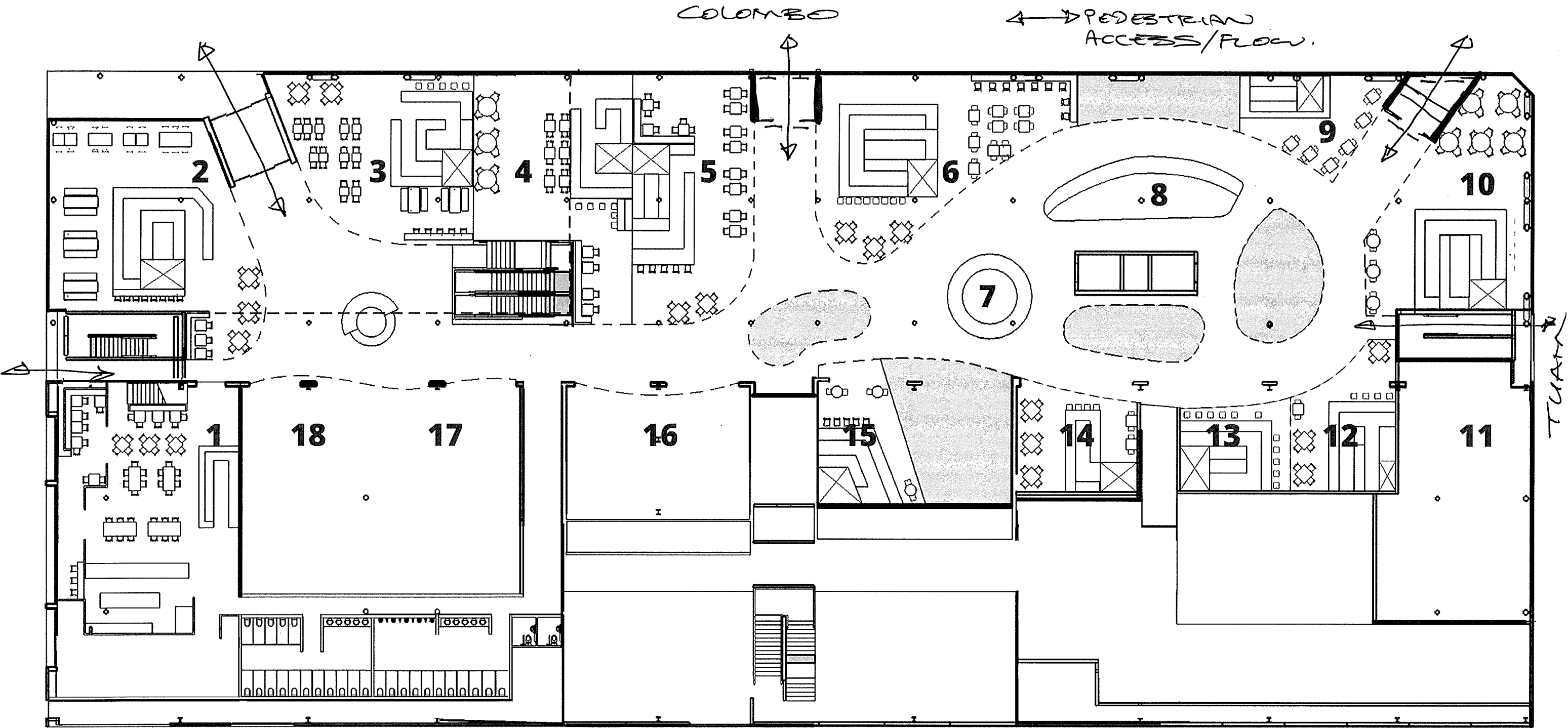
UNIT 12 - 60sqm

UNIT 15 - 55sqm

UNIT 18 - 120sqm

Christchurch City Council  
 Building the future since 1955  
 Stewart  
 RMA/2016/2863  
 15/11/2016

TOTAL LETTABLE APPROX. - 2000sqm





Scott Blair  
Christchurch City Council  
Via email: Scott.Blair@ccc.govt.nz

**Attention: Scott Blair**

25<sup>th</sup> October 2016

Dear Scott,

**RE: ADDENDUM TO RESOURCE CONSENT APPLICATION RMA/2016/2863**

I write to provide an addendum to the resource consent application (RMA/2016/2863) lodged with the Christchurch City Council seeking to establish of a three storey “entertainment exchange” housing a seven screen cinema complex with associated dining precinct on the site located at 617-649 Colombo Street, Christchurch.

This letter provides a commentary on the proposed Commercial Central City Business Zone rules that were confirmed in Decision 43 of Independent Hearings Panel on 27 September 2016. In relation to this application it is noted that decisions on Chapter 13 Central City (Stages two and three and relevant definitions and planning maps) were notified on the 27th September, whilst these rules are not beyond appeal they do have legal effect and must be considered alongside the operative City Plan rules.

**Zoning of Application Site**

Christchurch City Plan – The site is zoned Central City Business (Core) on Planning Map 39I of the Operative City Plan.

Proposed Christchurch Replacement District Plan –Commercial Central City Business (Core) on Planning Map CC.

Operative Replacement District Plan – N/A

**Replacement District Plan Compliance Assessment –Commercial Central City Business Zone Rules**

As mentioned previously, decisions on Chapter 13 Central City (Stages two and three and relevant definitions and planning maps) were notified on the 27th September, whilst these rules are not beyond appeal they do have legal effect and must be considered alongside the operative City Plan rules.

Therefore the proposal has been assessed against the relevant rules of pCRDP (Chapters 7 and 15) and the following commentary is provided. In summary the proposal does not introduce any new non-compliances over and

above those already addressed in the consent application and considered under the operative City Plan. However, it is noted that the urban design rule becomes a controlled activity under the proposed plan rather than assessed as a restricted discretionary activity under the operative plan.

### **Permitted Activity Tables**

With regards to permitted activities Rule 15.10.1.1 the following activities are permitted activities in the Commercial Central City Business Zone

- (P1) Retail Activities
- (P3) Entertainment facility

Based on the above the proposed cinemplex and dining precinct are permitted activities under Rule 15.10.1.1.

### **Controlled Activities**

With regards to controlled activities any new building listed as P1-P17 within the Central City Core is to be a controlled activity.

The proposal seeks to establish a new building within the Central City Core zone and therefore is a controlled activity. The proposal has been presented to the Urban Design Panel as well as consultation has been undertaken with Councils Principal Urban Design Mr Hugh Nicolson.

### **Built Form Standards**

#### 15.10.2.1 Building Setback and Continuation

a) The City Plan specifies sites within the core buildings shall be i) built up to road boundary except that where the allotment fronts more than one road boundary, buildings shall be built up to all boundaries of the allotment; ii) across 100% of the width of an allotment where it abuts all road boundaries (excluding access ways and service lanes), except that one vehicle crossing may be located on each road frontage of the site.

The proposed building is built up along the Colombo Street, Lichfield Street and Tuam Street road boundaries and across 100% of the site except for the main pedestrian entrance at ground floor level in the north-eastern corner of the site. Compliance with this rule is not achieved.

This non-compliance has been addressed under the operative City plan assessment which forms the original application.

#### 10.15.2.2 Verandahs

For sites denoted in 'Central City Active Frontages and Verandas and Building Setback Planning Map' requiring verandas every building shall provide either a veranda or means of weather protection. The application is not required to provide verandas in accordance with the associated Planning Map.

#### 15.10.2.3 Sunlight and Outlook for the Street

Buildings shall not project beyond a recession plane of 45° measured from the maximum road wall and angling into the site.

The proposed building is 19m in height and therefore cannot intrude the 45<sup>o</sup> recession plane as shown in the Appendix A elevations. Therefore the proposal complies with this standard.

#### 15.10.2.4 Minimum Number of Floors

For sites within the core and located on the relevant Planning Map 'Central City Core, Frame, Large Format Retail, and Health, Innovation, Retail and South Frame Pedestrian Precincts Planning Map' the minimum number of floors above ground level shall be two. The proposed building is three levels (inclusive ground level) as such compliance is achieved.

#### 15.10.2.5 Flexibility in building design for future use

There minimum distance between the top of the ground floor surface to the bottom of the first floor slab shall be 3.5m.

The ground floor of the proposed building has a floor to floor height of 5.04m between the ground and first floor level, thereby achieving compliance.

#### 15.10.2.6 Location of On-Site Parking

Car parking within the Core shall be located to the rear of, on top of, within or under building or when located on the ground floor of any building, not located within 10 metres of the road boundary.

The application site does not provide parking, as such, compliance is achieved.

#### 15.10.2.7 Fences and screening structures

The maximum height of any fence or screening structure located within 4.5 metres of a road boundary shall be as specified.

The proposal does not seek to erect any fences or screening structures within 4.5m of the road boundary, thereby achieving compliance.

#### 15.10.2.8 Screening of Outdoor Storage and Service Areas or Spaces

Any outdoor storage or service areas/spaces shall be i) located to the rear of the principal building on the site and ii) shall be screened from any adjoining site by landscaping, fence, wall or a combination of not less than 1.8m high.

An outdoor storage area is located to the rear of the buildings and is screened by the proposed building, ensuring compliance is met.

#### 15.10.2.11 Building Height

The maximum height of all buildings shall be 28m. The proposed maximum building height is 19m, thereby achieving compliance.

#### 15.10.2.12 Maximum Road Wall Height

The maximum height of the road wall of any building shall be: i. 21 metres in the area subject to a 28 metre height limit on the 'Central City Maximum Building Height Planning map'

The proposal has a maximum road wall height of 18m, therefore compliance is achieved.

15/11/2016

#### 15.10.2.13 Water supply for fire fighting

Provision for sufficient water supply and access to water supplies for firefighting shall be made available to all buildings via Council's urban reticulated system. The proposal has access to Council's reticulated water system and therefore achieves compliance.

Based on the above non-compliance the proposal is to be assessed as a controlled activity under Rule 15.10.1.2 (C1) and a restricted discretionary activity under Rule 15.10.1.3 (RD5).

### **Replacement District Plan Compliance Assessment – Commercial Transport Rules**

As mentioned previously, decisions on Chapter 13 Central City (Stages two and three and relevant definitions and planning maps) were notified on the 27th September, whilst these rules are not beyond appeal they do have legal effect and must be considered alongside the operative City Plan rules.

Therefore the proposal has been assessed against the relevant rules of pCRDP (Chapter 7) and the following commentary is provided regarding any modifications to the rules. In summary the proposal does not introduce any new non-compliance over and above those already addressed in the consent application and considered under the operative City Plan. Rather, the proposal as assessed under the proposed plan now complies with the high trip generator and therefore reduces the number of transport noncompliance's from three to two.

#### 7.4.2.1 Minimum number and dimensions of car parks required

f) Any activity containing buildings with a Gross Floor Area of more than 2500m<sup>2</sup> are required to provide mobility parking in accordance with Appendix 7.1 and be provided on the same site as the activity.

Based on a GFA of 10,304m<sup>2</sup> the proposal will require four accessible spaces, whereas none are proposed to be provided.

#### 7.4.2.2 Minimum number of cycle parking facilities required

For any activity provision shall be made for cycle parking in accordance with Appendix 7.2 and be provided on the same site.

Table 7.5 specifies food and beverage activities shall provide 1 visitor space/125m<sup>2</sup> PFA and 1/400m<sup>2</sup> PFA for staff. As mentioned above there is no visitor cycle requirement for buildings which have no road frontage setback, therefore only staff cycle parking is required to be assessed.

In the absence of specific / known tenants for the food and beverage tenancies, it is assumed for the purpose of this application that public floor area (PFA) will be around 67% of 4,548m<sup>2</sup> GFA for the food and beverage tenancies. Based on a PFA of 3,000m<sup>2</sup> (food and beverage) require eight staff cycle parks.

Table 7.5 specifies staff and visitor cycle parking provisions for cinemas at a rate of 1 cycle park per 60 seats. The seven cinema complex provides a total of 912 seats and therefore requires 15 staff cycle parks



Overall, the proposed entertainment exchange is to provide 15 + 8 = 23 cycle parks. The proposal does not provide any cycle parks and therefore does not comply with the requirements.

#### 7.4.2.3 Minimum Number of Loading Spaces

For any activity minimum loading provision shall be provided in accordance with Appendix 7.3 and be provided on the same site as the activity.

The proposal does not provide on-site parking and therefore is not required to provide a dedicated loading space. Therefore, compliance with the standard is achieved.

#### 7.4.2.10 High trip Generators

This rule applies to activities located outside the Central City, and activities within the Central City that are not exempt from this rule under ii. that exceed the following thresholds. ii. Within the Central City - *Permitted activities* are exempt from this rule.

*"For the purpose of this rule permitted activities must comply with the built form standards for the maximum building height for activity in the zone, any site coverage standards for the activity in the zone, and all the activity specific standards for the activity in the zone. Non-compliance with any other built form standards or being subject to an urban design assessment does not trigger the need to be subject to this rule".*

The proposal is not required to be assessed under the high trip generator rule as the proposed activity is listed in the permitted activity table for the Commercial Central City Business Zone and it does not breach the maximum building height or any site coverage rules.

Based on the above non-compliance the proposal is to be assessed as a restricted discretionary activity under Rule 7.4.1.3 (RD1).

### **Summary of Non-Compliances and Activity Classification**

The preceding compliance assessment identifies the following Proposed District Plan non-compliances

The overall activity status under the proposed City Plan is therefore for restricted discretionary activity.

#### **CCCB Zone Non-Compliances**

- 15.10.1.2 Controlled Activity; and
- 15.10.2.1 Building Setback and Continuation

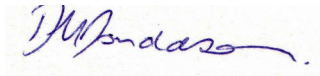
#### **Transport Zone Non-Compliances:**

- 7.4.2.1 Minimum number and dimensions of car parks required; and
- 7.4.2.2 Minimum number of cycle parking facilities required

I have not readdressed the objectives and policies for the proposed plan as these have been considered within the consent application lodged with Council.

If you have any questions please do not hesitate to contact either myself on 963 8706 or Ray Edwards on 963 8727.

Yours faithfully,



Damienne  
**Resource Management Planner**  
**URBIS TPD LIMITED**

APPENDIX B: Urban Design Panel Report (10 October 2016)

# Christchurch Urban Design Panel

## *Proposal Recommendations*

10 OCTOBER 2016

**By Email:** [Kevin.Arthur@calderstewart.co.nz](mailto:Kevin.Arthur@calderstewart.co.nz); [nicola@urbisgroup.co.nz](mailto:nicola@urbisgroup.co.nz);  
[Paul.Smith@calderstewart.co.nz](mailto:Paul.Smith@calderstewart.co.nz); [jeremyw@ignitearchitects.com](mailto:jeremyw@ignitearchitects.com); [gerryt@ignitearchitects.com](mailto:gerryt@ignitearchitects.com);  
[ray.edwards@urbisgroup.co.nz](mailto:ray.edwards@urbisgroup.co.nz)

Dear All

### **Urban Design Panel – Hoyts EntX Project – 617 Colombo Street:**

The above Panel considered your application on 5 October 2016. Please find below the confirmed comments from that meeting.

In response to the material circulated and the review meeting, the Panel thanks the applicant for their attendance and commends the applicant on locating an entertainment facility on an ideal site in the city adjacent the bus exchange and the South Frame and close to two public car parks. The Panel also commends the applicant on the intent to have ground floor activation of the street frontages, and the generous 5 metre ground floor height and provision of interior void spaces.

#### **A. KEY DESIGN AND CONSENT RECOMMENDATIONS:**

*"Recommendations on matters to be addressed for Panel support of the application"*

The Panel:

1. Encourages a high degree of public permeability and visual interest between Colombo Street and the interior to help to activate Colombo Street.
2. Supports and encourages the applicant to include a variety of tenancies like a street market, and to provide the opportunity for individual tenancies to 'personalise' their frontage along the 100 metre façade.
3. Encourage the applicant to further develop the rhythm of recessed windows and façade panels to be less repetitive and to create better visual interaction with the street particularly towards the northern end.
4. Recommend providing a public entrance from the southeast to improve pedestrian permeability particularly from the South Frame.
5. Given the large format of the scheme, recommends that it is important to retain and enhance the elements that modulate the scheme, including: bay windows; louvres; recessed windows; fine grain of materials; the canopy; recesses between the heavier and lighter weight facades on the north and south; and textured concrete elements.
6. Recommends further development of the west façade and in particular the corners of the building where visible from Tuam and Lichfield Streets given that they will be visible permanently on Tuam Street and in the long term on Lichfield Street.

# Christchurch Urban Design Panel

## *Proposal Recommendations*

### B. **SECONDARY RECOMMENDATIONS:**

*"Further improvements and value added recommendations"*

The Panel:

7. Encourages the applicant to provide a generous public space adjacent to the main north-eastern entrance in recognition of the number of potential users of the complex and to provide an opportunity for people to meet and wait before entering.
8. Recommends continuing the canopy soffit through to the interior of the building to blur the boundary between indoors and outdoors.
9. Suggests that where possible, the applicant considers providing daylight to work and circulation areas such as offices and stairs in favour back of house and toilet areas. For example, on level one the toilets and administration areas could be swapped.
10. Recommends that the applicant consults with the Matapopore Trust on how best to incorporate local cultural references and sustainability into the development.
11. Recognises that the signage needs to be developed in more detail and integrated with the scheme – the Panel is not opposed to the LED signage provided its use is associated with the entertainment complex and technical matters including traffic safety can be resolved.
12. Recommends consulting with a traffic engineer regarding the safety of the loading dock in relation to the Tuam Street Cycleway.

### **PLEASE NOTE:**

The Urban Design Panel is an advisory body only. The Panel has no statutory decision making powers. The Panel's recommendations are to assist you in the refinement of your development proposal and the reporting Council officer will take its advice into account when processing any resource consent applications. The decision on any application rests with the Council.

The Christchurch City Council understands that you may wish to refer to the Urban Design Panel recommendations in the promotion of your development proposal. Please note the comments are not intended for publication.

To further discuss the Panel's recommendations please contact Rachael Annan (Council Urban Design Panel facilitator) at [rachael.annan@ccc.govt.nz](mailto:rachael.annan@ccc.govt.nz).

Please feel free to contact me in regards to any administrative matters (as the Council Urban Design Panel administrator) at [mark.saunders@ccc.govt.nz](mailto:mark.saunders@ccc.govt.nz).

Yours sincerely



**Mark Saunders**

Committee & Hearings Advisor

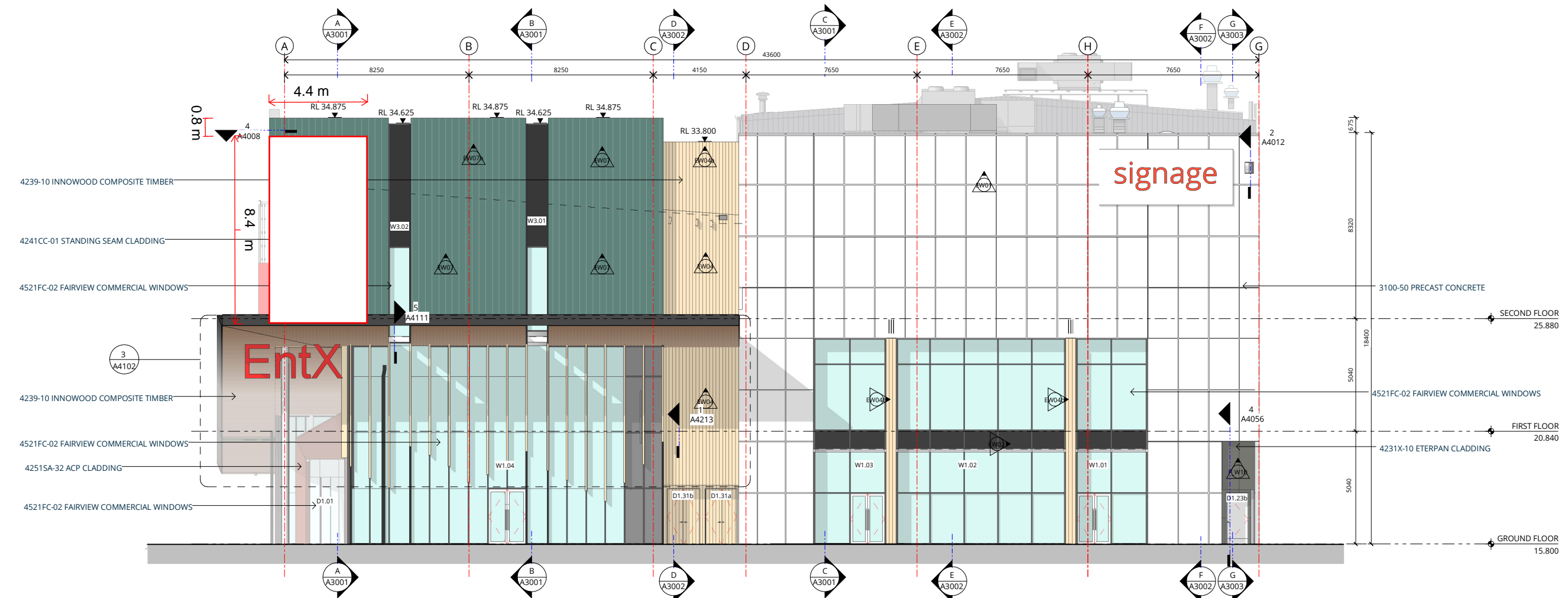
Community Support, Governance & Partnerships Unit  
Customer & Community Group

## APPENDIX C: Proposed Signage Plan

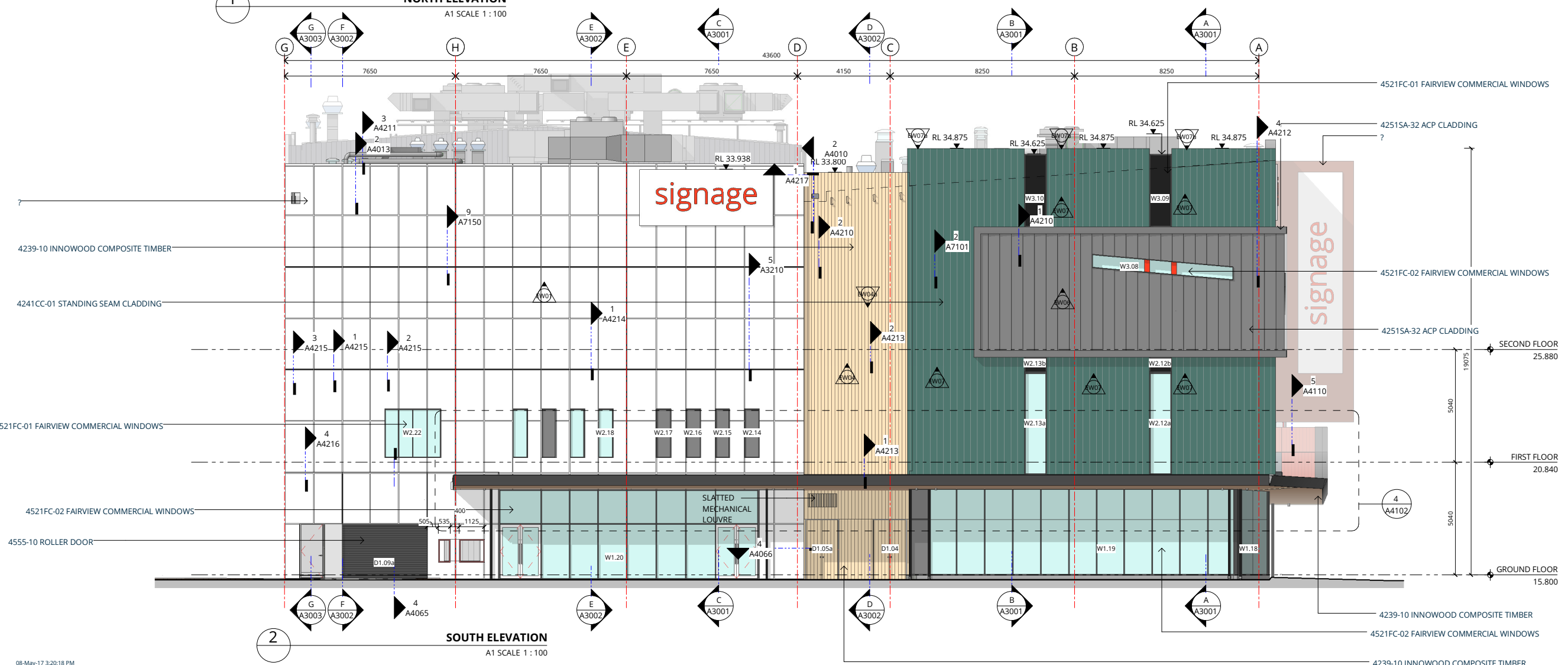
Rev	Description	By	Date
A	STAGE 1 CONSENT	IA	04/11/16
B	STAGE 2 CONSENT	IA	23/01/17
C	Fire RFI 2 & WIP		17.04.04
D	WIP Set		17.05.08

**UNCONTROLLED**

NOTES



**1 NORTH ELEVATION**  
A1 SCALE 1 : 100



**2 SOUTH ELEVATION**  
A1 SCALE 1 : 100

CONSULTANTS



PROJECT NAME  
**HOYTS EntX**  
617-649 COLOMBO STREET,  
CHRISTCHURCH  
Drawing Title  
**NORTH & SOUTH ELEVATIONS**

SCALE 1 : 100 @ A1

Architect	Drawn	Checked
IA	IA	IA
Project Commenced		Size

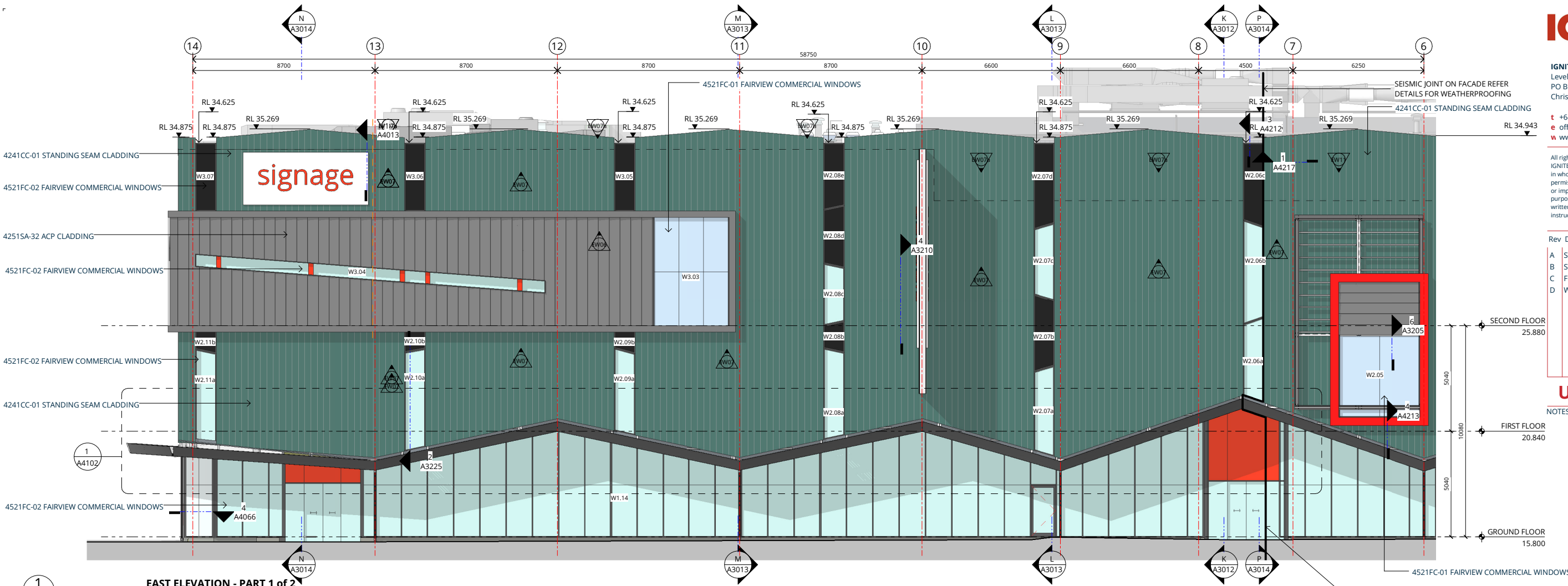
Project Number	Drawing Number	Rev
0281-002	A2001	D

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Rev	Description	By	Date
A	STAGE 1 CONSENT	IA	04/11/16
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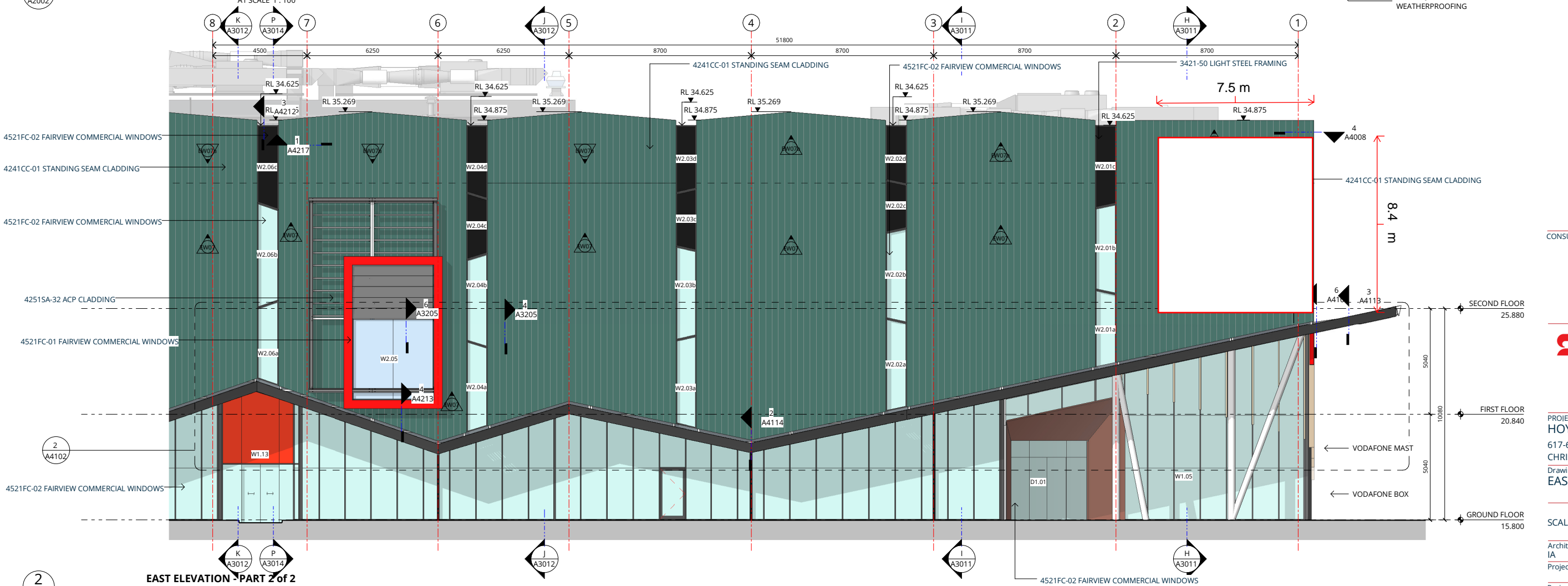
**UNCONTROLLED**

NOTES



**EAST ELEVATION - PART 1 of 2**

A1 SCALE 1 : 100



**EAST ELEVATION - PART 2 of 2**

A1 SCALE 1 : 100

CONSULTANTS



PROJECT NAME  
**HOYTS EntX**  
617-649 COLOMBO STREET,  
CHRISTCHURCH  
Drawing Title  
**EAST ELEVATION**

SCALE 1 : 100 @ A1

Architect	Drawn	Checked
IA	IA	IA
Project Commenced		Size

Project Number	Drawing Number	Rev
0281-002	A2003	D





Entx

CINEMA

## APPENDIX D: District Plan Compliance Assessment

6.3 Rules – Outdoor Lighting		
6.3.4 Activity status tables – Control of glare		
6.3.4.1.1 Permitted activities		
Activity	Comment	Activity Status
<p>a. The activities listed below are permitted activities if they meet the activity specific standards set out in the following table and the activity specific standards in Rule 6.3.5 Control of Light Spill.</p> <p>b. Activities may also be restricted discretionary or non-complying as specified in Rules 6.3.4.3, 6.3.4.5 and 6.3.5.3.</p> <p>P1</p> <p>Any activity involving artificial outdoor lighting, other than activities specified in Rule 6.3.4.5 NC1 or NC2.</p> <p>a. All fixed exterior lighting shall, as far as practicable, be aimed, adjusted and/or screened to direct lighting away from windows of habitual spaces of sensitive activities, other than residential units located in industrial zones, so that the obtrusive effects of glare on occupants are minimised.</p> <p>b. Artificial outdoor lighting shall not result in a greater than 2.5 lux spill (horizontal or vertical) into any part of a major arterial road or minor arterial road or arterial route identified in Appendix 7.12 where this would cause driver distraction.</p>	<p>The proposed signage is located opposite the central city bus exchange (to the east) and a department store (to the north). Therefore, the proposed signage is not lighting a habitual space and/or a sensitive area.</p> <p>Additionally, the application site is not located in the vicinity of a major arterial road and/or a minor arterial road and/or an arterial route.</p> <p>Overall, the proposal is in accordance with 6.3.4.1.1 Permitted Activities.</p>	<p><b>Permitted</b></p>

**6.3.5 Activity status tables – Control of Light Spill**

**6.3.5.1.1 Permitted Activities**

Applicable to and Rule	Comment	Activity Status
<p>a. The activities listed below are permitted activities if they meet the activity specific standards set out in the following table and the activity specific standards in Rule 6.3.4 Control of Glare.</p> <p>b. Activities may also be restricted discretionary or non-complying as specified in Rules 6.3.5.3, 6.3.5.5 and 6.3.5.</p> <p>P1</p> <p>Any activity involving outdoor artificial lighting.</p> <p>a. Any outdoor artificial lighting shall comply:</p> <ol style="list-style-type: none"> <li>i. With the light spill standards in Rule 6.3.6 as relevant to the zone in which it is located, and;</li> <li>ii. Where the light from an activity spills onto another site in a zone with a more restrictive standard, the more restrictive standards shall apply to any light spill received at that site.</li> </ol>	<p>All adjacent sites are of the same zoning as the application site, Commercial Central City.</p>	<p><b>Permitted</b></p>

**6.3.6 Activity status tables – Light Spill Standards by Zone**

**6.3.6.1 Permitted Activities**

Applicable to and Rule	Comment	Activity Status						
<p>a. The added horizontal or vertical illuminance from the use of artificial outdoor lighting must not exceed the limits in the following table of light spill standards by zone, when measured or calculated 2 metres within the boundary of the adjacent site.</p> <p>b. Where a site is divided by a zone boundary, each part of the site shall be treated as a separate site for the purpose of the standards continued on the following table of light spill standards by zone.</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;"></th> <th style="width: 25%;">Zone or scheduled activity</th> <th style="width: 20%;">Permitted lux spill (horizontal and vertical)</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">i.</td> <td>Commercial Central City Business Zone</td> <td style="text-align: center;">20.0</td> </tr> </tbody> </table>		Zone or scheduled activity	Permitted lux spill (horizontal and vertical)	i.	Commercial Central City Business Zone	20.0	<p>The application site is located in the in the Commercial Central City Business Zone and therefore is subject to a maximum lux spill of 20.0.</p> <p>Maximum daytime levels of this type of signage would be 5000 candela/m<sup>2</sup> and night-time would be 250 candela/m<sup>2</sup>. This equates to lux readings of 2.5 and 0.13 respectively when measured 22m away from the light source.</p>	<p><b>Permitted</b></p>
	Zone or scheduled activity	Permitted lux spill (horizontal and vertical)						
i.	Commercial Central City Business Zone	20.0						

6.8.4 Rules – Activity status tables - Signs		
6.8.4.1.1 Permitted Activities		
Activity	Comment	Activity Status
Any sign not specifically provided for as a permitted, controlled, restricted discretionary  P1 Relevant built form standards in Rule 6.8.5 (this should read 6.8.4.2.1)		
6.8.4.2 Rules - Built form standards		
6.8.4.2.1 Measurement of the area of a sign		
Applicable to and Rule	Compliance and Comment	Activity Status
a. For the purposes of measuring the area of any sign in accordance with Rule 6.8.4.2, a double-sided sign shall be measured as the area of one side only, being the larger of any one side (see Rule 6.11.8 - Diagram 2).	This proposal seeks permission for a single sided sign.	<b>N/A</b>
6.8.4.2.2 Traffic safety - applies to all signs		
Applicable to and Rule	Compliance and Comment	Activity Status
a. Any sign shall be located so as not to obscure or to detract from the interpretation of any traffic sign or controls	The proposed sig is located in the vicinity of traffic signals, however due to its placement it does not detract from the interpretation of any traffic signs or controls.	<b>Permitted</b>
b. No sign shall be located adjacent to a state highway or arterial road where all of the following criteria are met: <ul style="list-style-type: none"> <li>i. the road has a speed limit of 70km per hour or greater; and</li> <li>ii. the sign is located within a road boundary building setback required by a built form standard for the relevant zone; and</li> <li>iii. the sign is located within 100 metres (in urban zones) or 200 metres (in rural or open space zones) in front of any official regulatory or warning sign or traffic signal.</li> </ul>	In the vicinity of the application site, the District Plan classifies Lichfield Street as a central city local distributor and Colombo Street as a local street. Hence, this rule is not applicable.	<b>N/A</b>

6.8.4.2.3 Integration with building design		
Applicable to and Rule	Compliance and Comment	Activity Status
a. Any sign displayed on wall surfaces, including individual lettering, shall not obscure any window, door or architectural feature, visible from the exterior of the building.	The proposed signage does not detract from the façades it is mounted upon nor does it obstruct any doors or windows.  Therefore, the proposal complies with Rule 6.8.4.2.3, a.	<b>Permitted</b>
b. Where a sign, including a flag, extends over part of a transport zone the lowest part of the sign shall be located a minimum of 2.6 metres above ground level.	The proposed signage is flush with the supporting building and hence protruding over the adjoining footpath a negligible amount, especially considering the canopy located directly below.	<b>N/A</b>

6.8.4.2.4 Signs attached to buildings										
Applicable to and Rule	Compliance and Comment	Activity Status								
a. For signage on heritage items and in heritage settings the rules in Chapter 9 also apply.	The proposed sign is not located on a heritage building.	N/A								
b. The maximum area and height of signs shall be as follows: <table border="1" data-bbox="113 658 652 974"> <thead> <tr> <th>Zone or scheduled activity</th> <th>Maximum total area of signs per building</th> <th>Maximum height above ground level at top of sign</th> </tr> </thead> <tbody> <tr> <td>Commercial Central City Business Zone</td> <td>Length along primary building frontage (m) x 0.5m</td> <td>9 metres or façade height whitherever is lower.</td> </tr> </tbody> </table> <p>Note: Primary building frontage, in relation to signs and signage only, means any building frontage facing towards a public road or customer parking area.</p>	Zone or scheduled activity	Maximum total area of signs per building	Maximum height above ground level at top of sign	Commercial Central City Business Zone	Length along primary building frontage (m) x 0.5m	9 metres or façade height whitherever is lower.	<p>The proposed signage has a total area of approximately 103.5m<sup>2</sup> (9m high and 11.5m long).</p> <p>The site at 617-649 Colombo Street has a primary building frontage of approximately 190m (measured along all three road frontages) and therefore according to the District Plan is permitted a total signage area of 95m<sup>2</sup>. The proposed 103.5m<sup>2</sup> sign exceeds this maximum.</p> <p>Furthermore, the proposed signage has a maximum height of approximately 18m above ground level. Hence considerably surpassing the 9m maximum.</p> <p>Therefore, the sign does not comply with Rule 6.8.4.2.4, b on two accounts.</p>	Discretionary		
Zone or scheduled activity	Maximum total area of signs per building	Maximum height above ground level at top of sign								
Commercial Central City Business Zone	Length along primary building frontage (m) x 0.5m	9 metres or façade height whitherever is lower.								
6.8.4.2.5 Projecting signs and signs attached to or on verandas										
Applicable to and Rule	Compliance and Comment	Activity Status								
<p>In addition to meeting the built form standards in Rule 6.8.5.4, signs mounted and affixed to or on verandas, signs mounted to the face of verandas, and signs projecting from the face of a building, shall also meet the following built form standards:</p> <p>a. Signs mounted and affixed to or on verandas (see Rule 6.11.8 - Diagrams 4 &amp; 5)</p> <table border="1" data-bbox="113 1556 652 1991"> <tbody> <tr> <td>Maximum projection into any transport zone</td> <td>2.5 metres</td> </tr> <tr> <td>Minimum height above ground level for the lowest part of the sign</td> <td>2.6 metres</td> </tr> <tr> <td>Maximum distance from veranda top surface</td> <td>1.2 metres</td> </tr> <tr> <td>Minimum setback from the face of the kerb</td> <td>0.5 metres</td> </tr> </tbody> </table>	Maximum projection into any transport zone	2.5 metres	Minimum height above ground level for the lowest part of the sign	2.6 metres	Maximum distance from veranda top surface	1.2 metres	Minimum setback from the face of the kerb	0.5 metres	The proposal does not seek to mount signage on a veranda.	N/A
Maximum projection into any transport zone	2.5 metres									
Minimum height above ground level for the lowest part of the sign	2.6 metres									
Maximum distance from veranda top surface	1.2 metres									
Minimum setback from the face of the kerb	0.5 metres									

<p>b. Signs mounted to the face of verandas (see Rule 6.11.8 - Diagram 4)</p> <table border="1" style="width: 100%;"> <tr> <td style="width: 50%;">Maximum height of sign display</td> <td style="width: 50%;">0.5 metres</td> </tr> </table>	Maximum height of sign display	0.5 metres	<p>The proposal does not seek to mount signage on a veranda.</p>	<p><b>N/A</b></p>												
Maximum height of sign display	0.5 metres															
<p>c. Signs projecting from the face of a building (see Rule 6.11.8 - Diagrams 6 &amp; 7)</p> <table border="1" style="width: 100%;"> <tr> <td style="width: 15%;">Signs Parallel to the Building Face</td> <td style="width: 45%;">Maximum projection from the face of the building.</td> <td style="width: 15%;">0.2 metres</td> <td style="width: 25%;"></td> </tr> <tr> <td rowspan="2" style="vertical-align: top;">Signs Perpendicular to the Building Face</td> <td rowspan="2" style="vertical-align: top;">Maximum projection from the face of the building</td> <td style="vertical-align: top;">Greater than 2.6m above ground level</td> <td style="vertical-align: top;">1.2 metres</td> </tr> <tr> <td style="vertical-align: top;">No greater than 2.6m above ground level</td> <td style="vertical-align: top;">0.2 metres</td> </tr> <tr> <td colspan="2" style="vertical-align: top;">Minimum setback from the face of the kerb</td> <td colspan="2" style="vertical-align: top;">0.5 metres</td> </tr> </table>	Signs Parallel to the Building Face	Maximum projection from the face of the building.	0.2 metres		Signs Perpendicular to the Building Face	Maximum projection from the face of the building	Greater than 2.6m above ground level	1.2 metres	No greater than 2.6m above ground level	0.2 metres	Minimum setback from the face of the kerb		0.5 metres		<p>The proposed signage will project up to 0.7 from the supporting building's façade.</p>	<p><b>Discretionary</b></p>
Signs Parallel to the Building Face	Maximum projection from the face of the building.	0.2 metres														
Signs Perpendicular to the Building Face	Maximum projection from the face of the building	Greater than 2.6m above ground level	1.2 metres													
		No greater than 2.6m above ground level	0.2 metres													
Minimum setback from the face of the kerb		0.5 metres														
<p>d. Signs shall not project forward of the face of the veranda on which they are located.</p>	<p>The proposed signage does not significantly project nor it is mounted on a veranda.</p>	<p><b>Permitted</b></p>														
<p>e. Projecting signs shall only be at right angles to the building face they are fixed to.</p>	<p>The proposed signage does not significantly project from the supporting building's façade and is mounted inline with the building's façade. Hence, the minor protrusion shall be at right angles to the face of the supporting building.</p>	<p><b>N/A</b></p>														



6.8.4.1.4 Discretionary activities		
Activity	Comment	Activity Status
<p>D1</p> <p>The signs in all zones, other than signs provided for in Rule 6.8.4.1.1 P11 or P15, Rule 6.8.4.1.3 RD2, RD3 or RD5, or Rule 6.8.4.1.5 CN1:</p> <ul style="list-style-type: none"> <li>a. Off-site signs, other than signs provided for by Rule 6.8.4.1 P2, P3, P4, P5, P6 P12, P13 or P16;</li> <li>b. Illuminated signs, including intermittently illuminated signs;</li> <li>c. Signs with moving components;</li> <li>d. Signs with changing images/digital signs; and</li> <li>e. Captive balloons or blimps.</li> </ul>	<p>This proposal seeks permission for a digital sign displaying advertisements and information in the Commercial Central City Business Zone (CB).</p>	<p><b>Discretionary</b></p>

Christchurch City Council,  
PO Box 73014,  
Christchurch 8154

Via email: [Scott.Blair@ccc.govt.nz](mailto:Scott.Blair@ccc.govt.nz)

Level 1, 189 Montreal Street,  
Christchurch 8011  
P: (03) 963 8727  
C: (029) 963 8727

PO Box 10-318,  
Christchurch 8145  
E: [ray@urbisgroup.co.nz](mailto:ray@urbisgroup.co.nz)  
W: [www.urbisgroup.co.nz](http://www.urbisgroup.co.nz)

**Attention: Scott Blair**

25<sup>th</sup> August 2017

Dear Scott,

**RE: REQUEST FOR FURTHER INFORMATION – RMA/2017/1365**

I write in response to your request for further information dated the 7<sup>th</sup> of July 2017. I apologise the for delay in providing this response however the issues raised in the RFI are quite comprehensive and necessitate a considered response. The following response is provided in five sections being:

1. A general commentary on the issue of potential effects of roadside advertising on road safety;
2. A general discussion of the unique characteristics of the Colombo/Lichfield intersection compared to other urban intersections located within the central city;
3. Examples of moving image signage provided in locations where there is a significantly more intensive traffic environment compared to the Colombo/Lichfield intersection;
4. A direct response to the matter raised by Mr Turner in the RFI, and;
5. Some suggested conditions of consent.

**General Commentary**

Before responding directly to the RFI, we would like to record that we note in the RFI the references made to published research on the issue of the effects of advertising signage on road safety. Mr Turner has referred to the work of Mr Wachtel in particular. We have undertaken our own review of the information that has been reviewed by Mr Turner and we attach to this RFI response our comments on the referenced publications. We make the following summary comments on the referenced research:

1. The majority of current research focuses on billboards in a highway location and only considers their impact on motorists. The Colombo Street/ Lichfield Street intersection is an entirely different traffic environment and we have been asked to provide a safety assessment for cyclists and pedestrians. It follows that, overall, the referenced research is of limited relevance to this proposal;
2. The research again suggests that motorists have spare cognitive capacity in most driving situations, and this spare capacity can be used for to non-essential tasks such as viewing roadside signage;
3. It is quite clear that there is no proven link between roadside advertising and potential effects on road safety.
4. However, we accept that a cautious approach should be taken given the newness of the digital billboard technology and the somewhat unique nature of the traffic environment outside the site and, as discussed in the meeting on Monday 7<sup>th</sup> of August, we volunteer a Section 128 monitoring condition to enable regular measurement and evaluation of actual effects rather than unnecessarily constraining the proposed activity based on perceived effects.
5. A final point to note is the site's location within a commercial part of the Central City and particularly the lack of residential dwellings in the immediate vicinity. This results in the Sign adding to the vibrancy and regeneration of the city without comprising any activities occurring in the surrounding environment. Furthermore, due to the application site's location within the centre of the city it effectively contributes towards the reestablishment of the City's core.

### **The Lichfield Street / Colombo Street Intersection**

As noted above, currently available research into the impact of digital signage specifically in an urban environment is exceedingly limited. We accept that caution needs to be exercised when installing such a sign and the location needs to be carefully selected. The Colombo Street / Lichfield Street intersection has been identified as an ideal location to introduce this innovative technology into Christchurch for the following reasons.

1. The Colombo Street / Lichfield Street intersection is located in a low speed environment with the posted speed limit being 30 km/hr on all approaches. The road in the immediate vicinity is designed to encourage traffic calming predominantly by way of narrow lane widths. The low operating speed of the junction improves its safety and reduces the potential impact of human error (e.g. a car crash at 30km/hr is considerably less likely to result in injury than a car crash at 50km/hr).
2. Additionally, the volume of traffic on Colombo Street and Lichfield Street in this vicinity is anticipated to be relatively low. The narrow and low speed nature of the two roads discourages drivers from using them

unless accessing a facility in the immediate area, with the majority of traffic using alternative routes to cross the Central City. It is unlikely that these two roads will ever carry traffic volumes in the future akin to what they carried in the past.

3. With regards to pedestrians it is noted the traffic lights have a *Barnes-Dance* phase - this phase solely allows for pedestrian movement with vehicles on all approaches remaining stationary. This provides pedestrians with a safe crossing opportunity in all directions and removes the potential conflict caused by pedestrians crossing the path of left turning vehicles. Additionally, the footpaths in the immediate vicinity are of a considerable width, capable of accommodating a high pedestrian flow and providing space for pedestrians to stop without causing a significant obstruction.

### **Other Examples of Moving Image Signage**

We provide on the following pages some examples of animated/digital billboards in busy urban settings. All of the examples are located in areas of high pedestrian activity and within close proximity of road controls/intersections. The examples include the world's biggest digital billboard in New York's Time Square, stretching the whole block between 45th and 46th Street on Broadway (approximately 100m in length and eight storeys high). Approximately 300,000 pedestrians pass through Times Square every day. It is also clear that pedestrian and motorised traffic volumes in all of the examples are significantly higher than the Christchurch site.

Iconic billboards such as Times Square and Piccadilly Circus are on a much larger scale than is being proposed for the corner of Colombo and Lichfield Street, however, they demonstrate the contribution such installations can make to the vibrancy and character of an inner-city location, creating a modern and visually interesting environment that contributes to the city's atmosphere. Furthermore, the placement of these billboards (high above shop facades and in close proximity of intersections) is identical to what is being proposed and demonstrates the effectiveness of the mitigation measures discussed in the resource consent application.

Despite these examples, the uncertainty of the digital screen's potential impact is acknowledged, hence the Section 128 monitoring condition has been volunteered.



Times Square, New York (world's largest digital advertisement)



Piccadilly Circus, London



Yonge-Dundas Square, Toronto



Shibuya Crossing, Tokyo

## **RFI Response**

Turning to the matters raised in the RFI itself, the requested information is stated in italics below immediately followed by the provided response.

### ***Formal request for information 1:***

***Please provide information about the nature of the static images and moving images that will be displayed on the sign in particular:***

- a. transition times for static images;***
- b. the length for short movie clips and over what hours these will be displayed; and***
- c. clarification on whether the same image or video will wrap around the corner sign or there will be two separate images on each side of the corner (or a combination of both).***

This application seeks consent for a digital display that may exhibit both moving and static images:

- For static images, the screen shall feature clips with a minimum duration of 5 seconds. This is akin to what operates on number of digital billboards located citywide.
- For moving image clips, it is proposed to operate with a maximum duration of 60 seconds (this market typically sells advertising at 15, 30 and 60 seconds duration).
- It is proposed there is no restriction on the screens hours of operation.
- Finally, the entire digital screen may feature one continuous image, effectively wrapping around the corner of the building, or two separate images one on the Colombo Street façade and one on the Cashel Street façade. Again, this will operate with a maximum clip duration of 60 seconds.

### ***Formal request for information 2:***

***Please expand on the road safety assessment of environmental effects to include an analysis of the potential effects on cyclists – refer to the above discussion for context.***

It is acknowledged the original resource consent application did not consider cyclists separately from motorised traffic; however, the majority of the impact assessment provided for motorised vehicles is also applicable to cyclists. Dedicated on-street cycle lanes are provided on the northbound Colombo Street approach to the Colombo/Lichfield intersection. On the other three approaches to the intersection the cyclist shares the through traffic lane with motorised vehicles, and then sits ahead of the motorised vehicles if stopped at the traffic lights.

As was stated with regards motorised vehicles, the location (specifically the height) of the proposed signage mitigates the likelihood of confusion and/or distraction at the Colombo Street/ Lichfield Street intersection. Additionally, cyclists are likely to act in a similar way to motorists, prioritising essential information (traffic signals, the presence of pedestrians and other vehicles) over the nonessential information (e.g. shop displays, landscaping and architectural features). This is something cyclists (and motorists) are constantly doing, filtering out information relevant to the task of cycling (or driving) and focusing on the relevant information (e.g. light signal). The proposed signage is non-essential information, and due to its non-conflicting location (high above the traffic lights and partially concealed by a solid canopy) it will not hinder the interpretation of essential information.

Furthermore, the proposed signage is located in a low speed zone (30km/h), hence creating a relatively safe and cycle friendly environment. This does not necessarily aid in mitigating the impact of the proposed signage on the cyclists, however it does result in cyclists being anticipated and considered by drivers therefore reducing the prospect of vehicle/cyclist conflicts.

Research relating to the impact of digital displays predominantly focuses on motorists, therefore it is difficult to provide evidence with regards the impact on cyclist behaviour and safety. This lack of information is acknowledged and therefore a Section 128 review condition is offered to ensure the safe operation of the transport network.

***Formal request for information 3:***

***Please expand on the road safety assessment of environmental effects to include an analysis of the potential effects on pedestrians – refer to the above discussion for context.***

As was stated previously, the majority of research into the safety impacts of digital displays focuses on motorist's behaviour. There is therefore little available literature when considering the potential effects on pedestrian safety, except through the extrapolation of the effects on motorists (e.g. a distracted motorist is more likely to be involved in a collision). However, with such contradictory conclusions drawn from current research, when considering the extent of the impact of digital displays on motorists it seems fanciful to attempt to draw conclusions from it with respect to pedestrian safety.

The area within which the proposed signage is to be located has a high pedestrian presence that will continue to grow as the central city gradually reopens. Pedestrians are constantly presented by distractions, shop displays, street furniture, other pedestrians, surrounding architecture and building works, however, they have the ability to prioritise this information and act accordingly.



The Colombo/Lichfield intersection provides an important pedestrian link, connecting the bus exchange, parking facilities and shopping areas (of particular note the department store Ballantynes). A 'Barnes-dance' pedestrian only phase has been incorporated into the signal cycle to accommodate this high pedestrian demand and to permit the exclusive movement of pedestrians in all directions (i.e. diagonal crossing is possible). The signalised nature of the junction controls pedestrian movements, ensuring there is a safe crossing opportunity (and here we note that there has been only one reported crash involving pedestrians at the intersection in the last five years, and the nature of the crash would have most unlikely to have been influenced by any type of signage such as that proposed because the motorist was at fault).

Further, pedestrians are most likely to observe the proposed signage during the delay they occur when waiting to cross the intersection and not when crossing. Additionally, sufficient space is available on the footpath to accommodate onlooking pedestrians whom wish to stop and observe the proposed signage.

For the reasons stated above, and the lack of substantiated evidence stating a risk to pedestrian safety, is it considered the negative effects will be negligible. Furthermore, the suggested Section 128 review condition has been volunteered to ensure a high level of safety is preserved.

### **Suggested Draft Conditions**

We provide below some suggested draft conditions that have been prepared subsequent to our recent meeting with yourself and Mr Turner & Ms. Jodi Enright of Stantec. The conditions take into account the key issues/concerns raised in the meeting and are based upon conditions that have been attached to previous resource consents involving digital signage in Christchurch City. Please note, the method of monitoring that was discussed and provisionally agreed upon during our meeting at the Council on Monday the 7<sup>th</sup> of August is included as condition 10.

1. The development shall proceed in accordance with the information and plans submitted within the application, including further information submitted on the xxx of August 2017. The Approved Consent Documentation has been entered into Council records as RMA/2017/1365 (xxx pages).
2. Moving and still images shall be displayed on the sign.
3. The minimum duration of each advert shall 5 be seconds.
4. The maximum duration for each advert shall be 60 seconds.

5. There shall be no sound equipment associated with the sign and no sound equipment is to be installed as part of the screen.
6. Any content on the screen shall be consistent with the guidelines contained in Section 3 “*Animated, flashing and variable message signs*” of the NZTA Traffic Control Devices Manual, Part 3 Advertising Signs.
7. The condition and appearance of the Sign will be maintained at all times. In the event of sign failure, the sign shall default to either black or white screen.
8. The sign shall not exceed 20 lux of light when measured 22 metres away from the sign. The luminance of the sign shall be audited by an appropriately qualified lighting engineer/designer within 7 days of installation and operation of the sign to confirm maximum luminance levels in condition 8 are not being exceeded. Confirmation of this is to be forwarded to the Head of Resource Consents, Christchurch City Council within 48 hours of the audit being completed.
9. In accordance with s. 128 of the Resource Management Act 1991. The Christchurch City Council may serve notice on the Consent Holder of its intention to review, in whole or in part, the conditions of this consent, to deal with any adverse effect on the environment which may arise from the exercise of the consent and which it is appropriate to deal with at a later for the following purposes:
  - At semi-annual intervals (6 monthly) the Crash Analysis System (CAS) managed by the New Zealand Transport Agency shall be reviewed by an appropriately qualified transport engineer identifying any injury crashes that have occurred where distraction from the sign and/or surrounding sign equipment is assessed to be a directly contributory factor. The area of interrogation shall cover at least 90m back from the Lichfield Street / Colombo Street intersection on all approaches thereby capturing reported crashes in the vicinity of the Sign and excluding crashes from neighbouring junctions.

Additionally, the Council’s closed-circuit television (CCTV) of the junction shall be recorded for a random 24-hour period within each 6-monthly block. The footage shall then be reviewed by an appropriately qualified transport engineer, identifying near misses and/or changes in driver, cyclist and pedestrian behaviour due to the presence of the sign.

Both sources shall be used to determine if the presence of the billboard has altered the operation of the junction in a negative manner. The first review shall occur 6 months post the installation of the Sign and shall be conducted for a period of 5 years. If after five years no significant negative impact has been identified the s. 128 shall be removed and monitoring shall cease.

I trust you now have sufficient information such that the processing of the application may continue.

Yours faithfully,

**URBIS TPD LIMITED**



**Ruth Dunn**

**Graduate Transport Engineer**

## **Summary of Research**

### **Investigating driver distraction: the effects of video and static advertising**

By M Chattington, N Reed, D Basacik, A Flint, A Parkes (TRL)

Transport Research Laboratory for Transport for London (TfL) and London Road Safety Unit

June 2009

#### **Description of Study:**

A driving simulator and integrated eye-tracking system to compare driving behaviour across a number of experimental advertising conditions. Forty-eight participants took part in the trial, with three factors examined; Advert type, position of adverts and exposure duration to adverts.

#### **Conclusion:**

The results indicated that when passing advert positions, drivers:

- Spent longer looking at video adverts;
- Glanced at video advert more frequently;
- Tended to show greater variation in lateral lane position with video adverts;
- Braked harder on approach to video adverts;
- Drove more slowly past video adverts.

#### **Points of note:**

- 30mph speed environment

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### **Driver Distraction from Roadside Advertising: The clash of road safety evidence, highway authority guidelines and commercial advertising pressure**

By Time Horberry (University of Queensland, Brisbane), Michael A. Regan (INRETS), Jessica Edquist (Monash University Accident Research Centre, Vic)

#### **Description of Study:**

Providing a review of three conflicting data sources/stakeholders: road safety evidence produced by the research community, the role of road authorities to develop balanced, defensible and robust guidelines to regulate advertising, and the role of commercial advertisers and advertising bodies who seek to maximize the exposure of their billboards.

#### **Conclusion:**

- Lack of both comprehensive research evidence and international regulatory agreement, road authorities may therefore be justified in using the best research information available (albeit incomplete) coupled with engineering judgement.
- Higher crash rates associated with the presence of advertisements around intersections.

- Emphasis should be placed on advertisers to prove that a potential roadside advertisement is safe, rather than purely on road authorities to prove it is unsafe.
  - Annual approval permits are often a good idea – so road authorities or researchers can yearly assess the impact of the advertising.
  - There is a paradox confronting researchers in this area: billboards are designed to attract visual attention, and it is expected that they would be a source of distraction; indeed...many studies do show that drivers direct visual attention to them as they drive. But such studies have often found little impact upon driving performance. This might be due to 'spare' driver visual capacity – when a driver's cognitive demand increases then they might look less at the advertisement. However, such advertisements are often placed in high workload situations (e.g. junction) where the advertiser is expecting more eyes to look at them.
  - Recommended a restriction of advertisements at times when driver decision, action points and cognitive demand are greatest – such as at freeway exits/entrances, lane reductions, merges and curves.
- 

### **Advertising billboards impair change detection in road scenes**

Jessica Edquist, Tim Horberry, Simon Hosking and Ian Johnston

Monash University Accident Research Centre

November 2011

#### **Description of Study:**

In a controlled experiment, inexperienced, older, and comparison drivers searched for changes to road signs and vehicle locations in static photographs of road scenes.

#### **Conclusion:**

On average, participants took longer to detect changes in road scenes that contained advertising billboards. This finding was especially true when the roadway background was more cluttered, when the change was to a road sign, and for older drivers.

#### **Points of note:**

- The study was based purely on the interpretation of static images providing limited insight. Additionally, participants were not undertaking the task of driving (either via simulator or reality) further restricting the validity of the study.
- 

### **Effects of Electric Billboards on Driver Distraction**

Tania Dukic, Christer Ahlstorm, Christopher Patten, Carmen Kettwich, Katja Kircher

Swedish National Road and Transport Research Institute and Light Technology Institute

May 2012

#### **Description of Study:**

A total of 41 drivers were recruited to drive an instrumented vehicle passing 4 of the electronic billboards during day and night conditions. A driver was considered visually distracted when looking at a billboard continuously for more than 2s or if the driver looked away from the road for a high percentage of time. Dependant variables were eye-tracking measures and driving performance measures.

**Conclusion:**

Electronic billboards have an effect on gaze behaviour by attracting more and longer glances than regular traffic signs. Whether the electronic billboards attract much more attention and constitute a traffic safety hazard cannot be answered conclusively based on the present data.

**Points of note:**

- None of the assessed billboards contained animated images, only static displays.
- The billboards were all located in high speed environments (80km/h+).
- Uncertainty with regards how driving behaviour and gaze behaviour may change in more or less complex situations.
- Uncertainty with regards to what extent billboards attract attention and hence if it constitutes a traffic safety hazard.

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**Austrroads Research Report: Impact of Roadside Advertising on Road Safety**

By Paul Roberts, Kathy Boddington and Libby Rodwell

January 2013

**Description of Study:**

The aims of this project were to; firstly, review the extant literature on the distraction risk associated with roadside advertising and to communicate this. The second aim was to document and review the existing guidelines across road agencies so that inconsistencies and gaps could be identified. Finally, these outputs were to be used to inform guiding principles and make guidance recommendations that can be used to create guidelines and to harmonise guidelines across road agencies.

**Conclusion:**

Most drivers, in most situations, most of the time, probably possess substantial spare cognitive capacity for the processing of driving-irrelevant information.

While attention may be less likely to be captured by irrelevant material in a demanding driving situation, it is clear that in some driving situations it is likely that movement or changes in luminance will involuntarily capture attention and that particularly salient emotional and engaging material will recruit attention to the detriment of driving performance...Where this happens in a driving situation that is also cognitively demanding, the consequences for driving performance are likely to be significant.

There is compelling evidence that distraction is a major contributor to crashes. However, studies providing direct evidence that roadside advertising plays a significant role in these distractions based on crashes are currently not

available. The studies that have been conducted show convincingly that roadside advertising is distracting and that it may lead to poorer vehicle control.

**Points of note:**

- Street level advertisements attract more attention.
- Video advertising was significantly more distracting than static advertising.
- Individuals have been found to spend more time processing advertisements in less demanding scenarios (e.g. motorway compared to urban)
- Few studies attempt to investigate the impact of roadside advertising on actual crash rates.
- NZTA advise advertising signs should not be located within 100m of an intersection in an urban environment.

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**Compendium of Recent Research Studies on Distraction from Digital Signs**

By Jerry Wachtel, CPE President, The Veridian Group, Inc. Berkeley, California

June 2013

**Description of Study:**

A literature review of 11 relevant research articles

**Conclusion:**

These studies found that outdoor advertising signs did attract drivers' attention, and that more dramatic and salient signs attracted attention longer, and with more frequent glances. This attention was often captured in a 'bottom up' psychophysical process, where the driver attended the sign unintentionally and unconsciously, the eyes captured involuntarily by the sign's brightness, conspicuity or movement. While some found a relationship between advertising signs and crashes...others did not, demonstrating the great difficulty involved in measuring, or controlling, the many variables that might affect the outcome of studies.

**Summary**

- Current research inconclusive.
- No studies undertaken in a 30km/h zone.
- Studies predominately focus on uncomplex, high speed, situations (e.g. motorway driving).
- Relationship between distraction and safety not quantified.
- Limited research available for animated signs due to the newness of the technology.

### **General Conditions**

1. The development shall proceed in accordance with the information and plans submitted within the application, including further information submitted on the **insert date**. The Approved Consent Documentation has been entered into Council records as RMA/2017/1365 (xxx pages).
2. Any content displayed on the screen shall comply with the Advertising Standards Authority Advertising Code of Practice and the Broadcasting Act 1989.
3. Any content displayed on the screen should not contain any New Zealand road signs that are specified in the Traffic Control Devices (TCD) Manual or the Manual of Traffic Sign and Marking (MOTSAM).
4. Multiple consecutive adverts for the same products is prohibited.
5. There shall be no sound equipment associated with the screen and no sound equipment is to be installed as part of the screen.
6. The screen shall not be made of a material that is reflective to other light sources such as vehicle headlights.
7. The condition and appearance of the screen will be maintained at all times. In the event of screen failure, the screen shall default to either black or white.

### **Screen Illumination**

8. The screen shall not exceed 20 lux of light when measured 22 metres away from the screen.
9. The brightness of the screen shall be adjusted in response to changes in ambient light levels so that the images are not unreasonably bright for the safety of motorists. The maximum luminance (brightness) levels are:
  - a) 5000ch/m<sup>2</sup> during the day-time,
  - b) 600cd/m<sup>2</sup> at one hour either side of dawn and dusk, and;
  - c) 250cd/m<sup>2</sup> at night.
10. The luminance of the screen shall be audited by an appropriately qualified lighting engineer/designer within 7 days of installation and operation of the screen to confirm maximum luminance levels in Conditions 8 and 9 are not being exceeded. Confirmation of this is to be forwarded to the Head of Resource Consents, Christchurch City Council within 48 hours of the audit being completed.



### **Static Screen Content**

11. Any images or messages displayed on the screen shall contain no movement of any type (i.e. no animation, flashing, scrolling, intermittent or full-motion video shall be displayed).
12. Any static images or messages that are displayed on the screen shall have a minimum duration of 8 seconds.
13. There shall be no transitions between static images, apart from either:
  - a) An immediate change;
  - b) A maximum cross-dissolve period between images of 0.5 seconds.
14. Between transitions there will be no flashing or blinking.
15. No more than one advert shall be displayed on the screen at any one time.

### **Animated Screen Content**

16. Any moving images that are displayed on the screen shall be limited to a minimum of 8 seconds and a maximum of 20 seconds duration for the first two years of operation.
17. Any content on the screen shall be consistent with the guidelines contained in Section 3 "*Animated, flashing and variable message signs*" of the NZTA Traffic Control Devices Manual, Part 3 Advertising Signs.

### **Monitoring Programme**

18. Prior to the proposed screen become operational, the Applicant shall undertake a 'baseline' traffic safety analysis of the Colombo/Lichfield intersection including up 50 metres on each of the four approaches to the intersection. The analysis shall be undertaken by an appropriately qualified transport engineer and include (but not be limited to) information on the following matters:
  - a) An inventory of land use signage and potential other distractions to road users within 50m of the intersection.
  - b) Available traffic count data for all four approaches to the intersection. This should include information on daily and hourly traffic volumes and identify the busiest hourly periods across the week.

- c) Based on the data in b) above, peak hourly traffic counts for the two busiest one-hour periods identified across a given week. The peak hour counts are to include data collection of cyclists and pedestrian movements (including direction) at the intersection.
- d) The Crash Analysis System (CAS) managed by the New Zealand Transport Agency shall be reviewed to identify any injury crashes that have occurred where distraction from the sign and/or surrounding sign equipment is assessed to be a directly contributory factor. That either the Police crash report, including the officers comments or that of the witnesses or victims, needs to refer to the sign as a contributing factor, or that discussions with the Police officer who attended the crash needs to have identified the sign as a contributing factor. The area of interrogation shall cover at least 50m back from the Lichfield Street / Colombo Street intersection.
- e) The Council's closed-circuit television (CCTV) surveillance of the intersection shall be recorded for a 24-hour period that includes the two daily peak hours, and during a typical weekday period (ie not during school holidays). The same period to be used for all subsequent surveys. The footage shall then be reviewed to identify traffic conflicts (being collisions and near-misses between vehicles, cyclists and/or pedestrians). The methodology (survey and analysis) to be used for the traffic conflict analysis is to be in accordance with, or similar to, the British Traffic Conflict Technique (reference - [http://www.ictct.org/wp-content/uploads/LIB\\_Kraay\\_1982.pdf](http://www.ictct.org/wp-content/uploads/LIB_Kraay_1982.pdf) and paper by CJ Baguley (TRRL) titled The British Traffic Conflict Technique: State of the Art Report). This video footage is to be retained until the end of the monitoring programme.

#### 12 and 24 Month Traffic Surveys (Data Collection)

- 19. 12 and 24 months after the screen becomes operational, the Applicant shall repeat the traffic safety analysis as specified in Condition 18 for the purposes of evaluating any changes in road safety performance and road user behaviour at the intersection. These additional studies shall evaluate the following additional matters:
  - a) Any changes to the layout of the intersection following installation of the screen;

- b) Changes in traffic, pedestrian and bicycle flow conditions, and;
- c) Whether or not the number of serious and/or slight (moderate) traffic conflicts increases by more than 30% above the baseline level.

#### 12 and 24 Month Traffic Surveys (Analysis)

20. If the number of traffic conflicts after the 12 and 24 month surveys have been completed are:
  - a) Both less than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant is able to increase the maximum duration of moving images that are displayed on the screen to 60 seconds.
  - b) More than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow for either survey, then the applicant shall continue to operate the screen in accordance with conditions 16 and 17 above (i.e. operating a maximum video clip of 20 seconds).

#### 36 Month Traffic Survey (Data Collection and Analysis)

21. 36 months after the screen becomes operational, the Applicant shall repeat the traffic safety analysis as specified in Condition 18 for the purposes of further evaluating any changes in road safety performance and road user behaviour at the intersection.
22. If the screen has been operating at a maximum clip length of 60 seconds in accordance with Condition 20a), and the number of traffic conflicts after the 36-month survey is:
  - a) Less than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant is able to retain the maximum duration of moving images that are displayed on the screen at 60 seconds.
  - b) More than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant shall continue to permanently operate the screen in accordance with conditions 16 and 17 above (i.e. operating a maximum video clip of 20 seconds) and this monitoring programme will cease.
23. If the screen has been operating at a maximum clip length of 20 seconds in accordance with Condition 20b), and the number of traffic conflicts after the 36-month survey is:
  - a) Less than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant is able to retain the maximum duration of moving images that are displayed on the screen at 20 seconds.
  - b) More than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant shall continue to permanently

operate the screen in accordance with conditions 11 to 15 inclusive (i.e. static images) and this monitoring programme will cease.

#### 48 Month Traffic Survey (Data Collection and Analysis)

24. 48 months after the screen becomes operational, the Applicant shall repeat the traffic safety analysis as specified in Condition 18 for the purposes of further evaluating any changes in road safety performance and road user behaviour at the intersection
25. If the number of traffic conflicts following implementation of Condition 222a) (i.e. a maximum clip length of 60 seconds) after the 48-month survey is:
  - a) Less than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant is able to permanently retain the maximum duration of moving images that are displayed on the screen at 60 seconds and this monitoring programme will cease.
  - b) More than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant shall continue to permanently operate the screen in accordance with conditions 16 and 17 above (i.e. operating a maximum video clip of 20 seconds) and this monitoring programme will cease.
26. If the number of traffic conflicts following implementation of Condition 23a) (i.e. a maximum clip length of 20 seconds) after the 48-month survey is:
  - a) Less than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant is able to permanently increase the maximum duration of moving images that are displayed on the screen to 60 seconds in accordance with condition 20a) and this monitoring programme will cease.
  - b) More than 30% above the baseline number of traffic conflicts adjusted for changes in traffic flow, then the applicant shall continue to permanently operate the screen in accordance with conditions 11 to 135 inclusive (i.e. static images) and this monitoring programme will cease.
27. Irrespective of the outcome of the 12, 24, 36 and 48-month surveys, if there is one or more fatal crash or two or more serious crashes or five or more minor injury crashes that can be directly attributed to distraction caused by the operation of the screen over

the first 48 months, then the applicant shall be restricted to static images on the screen as soon as any of these crash totals are reached, in accordance with conditions 11 to 135 inclusive. In making such an assessment a crash may be disregarded, even if the operation of the sign was considered a key factor, if there are contributing factors, such as alcohol and drug levels above the legal limits that may have influenced the victim's behaviour such that the crash is likely to have happened irrespective of the operation of the sign.

#### **Section 128 Review of Consent Conditions**

28. In accordance with s. 128 of the Resource Management Act 1991 the Christchurch City Council may serve notice on the Consent Holder of its intention to review, in whole or in part, the conditions of this consent to deal with any adverse effect on the environment which may arise from the exercise of the consent and which it is appropriate to deal with at a later stage.